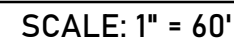
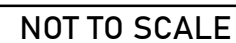
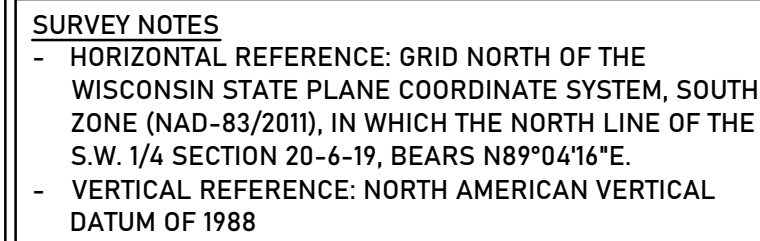


CITY OF WAUKESHA, WISCONSIN

14. CONTRACTOR SHALL NOTIFY CITY ENGINEERING DEPARTMENT FIVE (5) DAYS PRIOR TO WORK IN THE CITY RIGHT OF WAY.



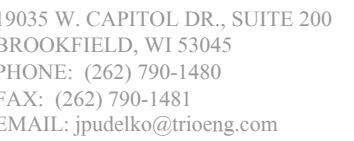
NOTE:
ALL SITE IMPROVEMENTS AND CONSTRUCTION SHOWN ON
THE PLANS SHALL CONFORM TO THE CITY OF WAUKESHA
DEVELOPMENT HANDBOOK & INFRASTRUCTURE
SPECIFICATIONS WHERE THE PLANS DO NOT COMPLY, IT
SHALL BE THE SOLE RESPONSIBILITY AND EXPENSE OF
THE DEVELOPER TO MAKE REVISIONS TO THE PLANS
AND/OR CONSTRUCTED INFRASTRUCTURE TO COMPLY.



BIELINSKI HOMES
1830 MEADOW LN., SUITE A
WAUKESHA, WI 53072
PHONE: (262) 548-5570

TRIO ENGINEERING, LLC
4100 N. CALHOUN ROAD, SUITE 300
BROOKFIELD, WISCONSIN 53005
PHONE: (262) 790-1480
FAX: (262) 790-1481

CIVIL	
T1	- COVER SHEET
C1.0	- EXISTING SITE PLAN
C1.1	- PROPOSED SITE PLAN
C2.0	- GRADING & DRAINAGE PLAN
C2-1-2.2	- STORMWATER FACILITY DETAILS
C3.0	- EROSION CONTROL PLAN
C4.0	- OVERALL SANITARY & WATER PLAN
C4.1	- SANITARY PLAN & PROFILE
C4.2	- WATERMAIN PLAN & PROFILE
C5.0	- ROAD & STORM PLAN & PROFILE
C5.1	- STORM SEWER PLAN & PROFILE
C6-0-6.1	- CONSTRUCTION DETAILS

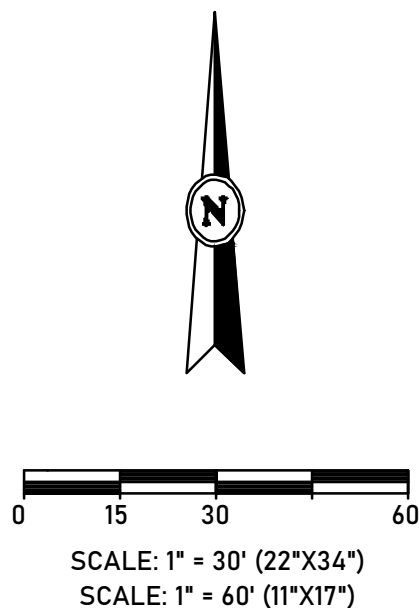


T1

X:\2025\25-09-966 RIVER ROAD VILLAS - WAUKESHA\DRAWINGS\CONSTRUCTION PLANS\RIVER ROAD VILLAS_CIVIL PLAN_SET_22X34.DWG

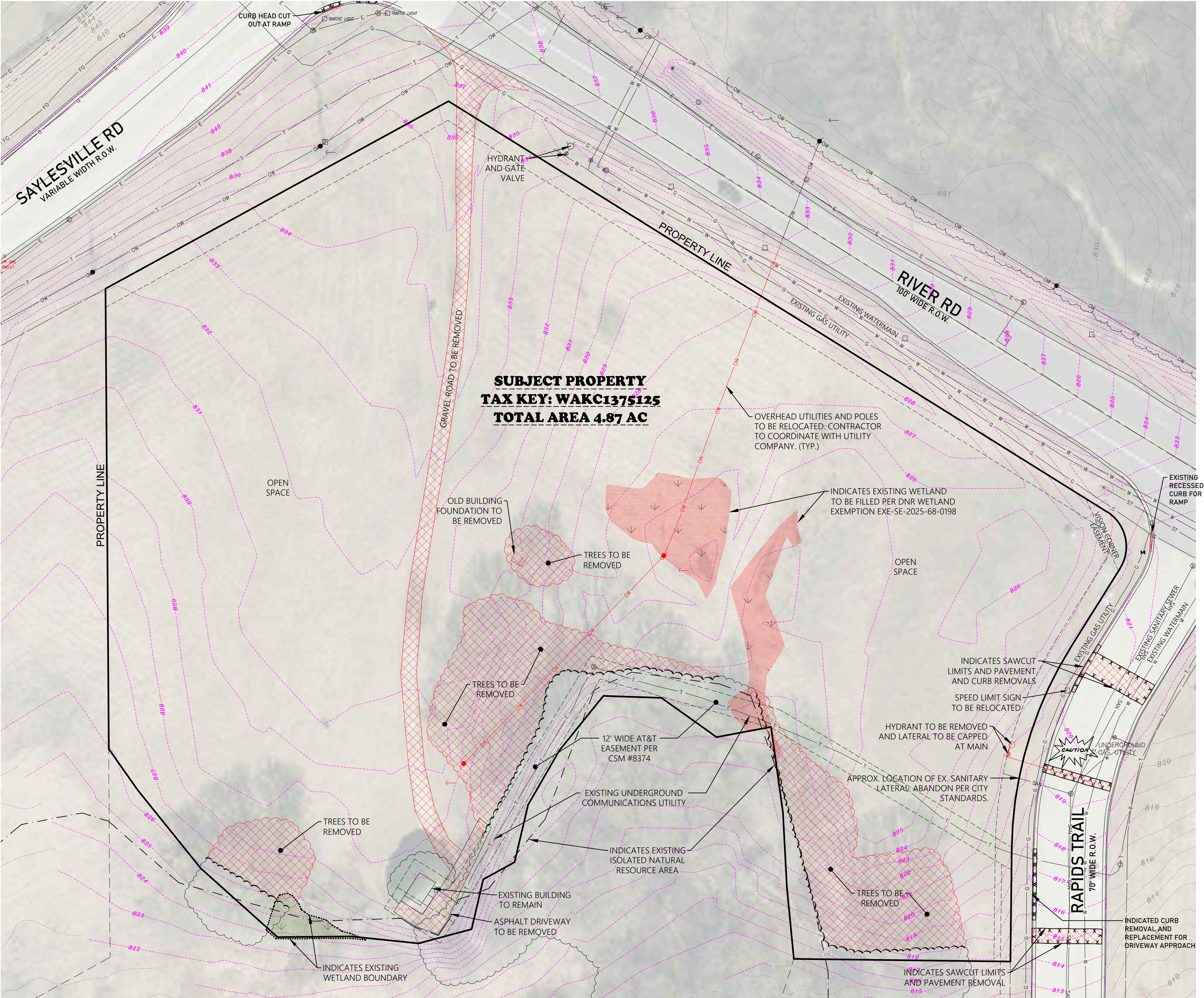
LEGEND:	
	- EXISTING CONTOUR
	- EXISTING SANITARY SEWER
	- EXISTING SANITARY MANHOLE
	- EXISTING WATER MAIN
	- EXISTING HYDRANT
	- EXISTING STORM SEWER
	- EXISTING STORM MANHOLE
	- EXISTING STORM INLET
	- EXISTING TRANSFORMER
	- EXISTING ELECTRIC PEDESTAL
	- EXISTING TELEPHONE PEDESTAL
	- EXISTING CATV PEDESTAL
	- EXISTING POWER POLE

NOTE:
LIMITS OF FINAL CITY STREET PAVEMENT AND CURB & GUTTER REMOVAL AND REPLACEMENTS TO BE MARKED BY CITY ENGINEERING STAFF IN FIELD. CONTRACTOR SHALL COORDINATE WITH THE CITY ENGINEERING DEPARTMENT.



811 CONTRACTOR IS REQUIRED TO CONTACT DIGGERS HOTLINE TOLL FREE TO OBTAIN LOCATION OF UNDERGROUND UTILITIES PRIOR TO COMMENCING THE WORK. WISCONSIN STATUTE 182.0715 REQUIRES MIN. OF 3 WORK DAYS NOTICE BEFORE YOU EXCAVATE. KNOW WHAT'S BELOW. **CALL BEFORE YOU DIG.**

NOTE:
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BROOKFIELD, WI 53045
PHONE: (262) 790-1480
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PROJECT:
RIVER ROAD VILLAS
CITY OF WAUKESHA, WISCONSIN
BY: BIELINSKI HOMES
1830 MEADOW LN., SUITE A
PEWAUKEE, WI 53072

REVISION HISTORY	
DATE	DESCRIPTION
05/12/25	PRELIMINARY PLANS
08/19/25	ROAD PROFILE REVISION
11/14/25	PER CITY COMMENTS

DATE:
NOVEMBER 14, 2025

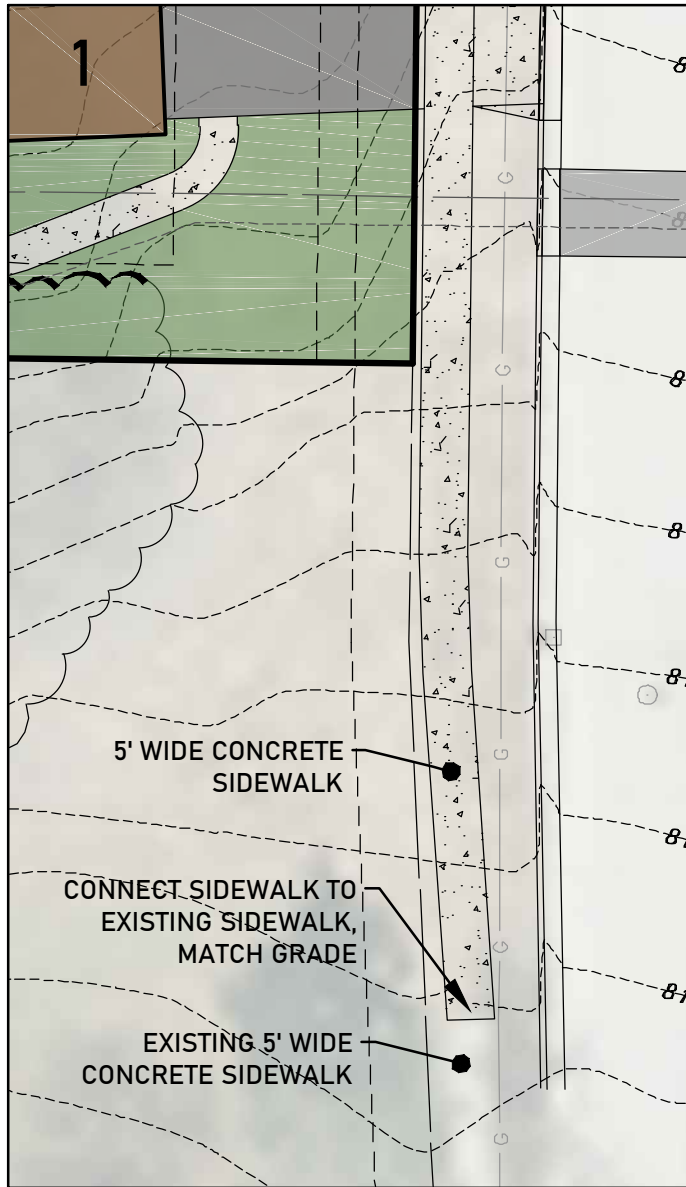
JOB NUMBER:
25-09-966

DESCRIPTION:
EXISTING
SITE PLAN

SHEET
C1.0

X:\2025\25-09-966 RIVER ROAD VILLAS - WAUKESHA\DRAWINGS\CONSTRUCTION PLANS\RIVER ROAD VILLAS_CIVIL PLAN SET_22X34.DWG

SITE DATA	
EXISTING ZONING:	B-5 (Community Business)
PROPOSED ZONING:	RD-2 (Two-Family Resident)
SETBACKS:	
FRONT:	25 FEET
REAR:	40 FEET
SIDEYARD:	10 FEET
PROPOSED LOT AREA DATA:	
TOTAL LOT AREA:	212,032 S.F. (4.87 Acres)
TOTAL BUILDING AREA:	42,272 S.F. (0.97 Acres) 19.9% of Lot
TOTAL PAVEMENT AREA:	47,782 S.F. (1.10 Acres) 22.6% of Lot
TOTAL OPEN SPACE AREA:	169,759 S.F. (3.90 Acres) 57.5% of Lot
PROPOSED UNITS:	24 UNITS
DENSITY:	4.93 UN/AC
PARKING DATA:	
REQUIRED PARKING SPACES:	2 Per Unit (48 Total)
PROPOSED PARKING SPACES:	
GARAGE SPACES:	48 SPACES (2 PER UN)
DRIVEWAY SPACES:	48 SPACES (2 PER UN)
SURFACE SPACES:	6 SPACES
TOTAL SPACES:	102 SPACES (4.25 SPACES/UNIT)



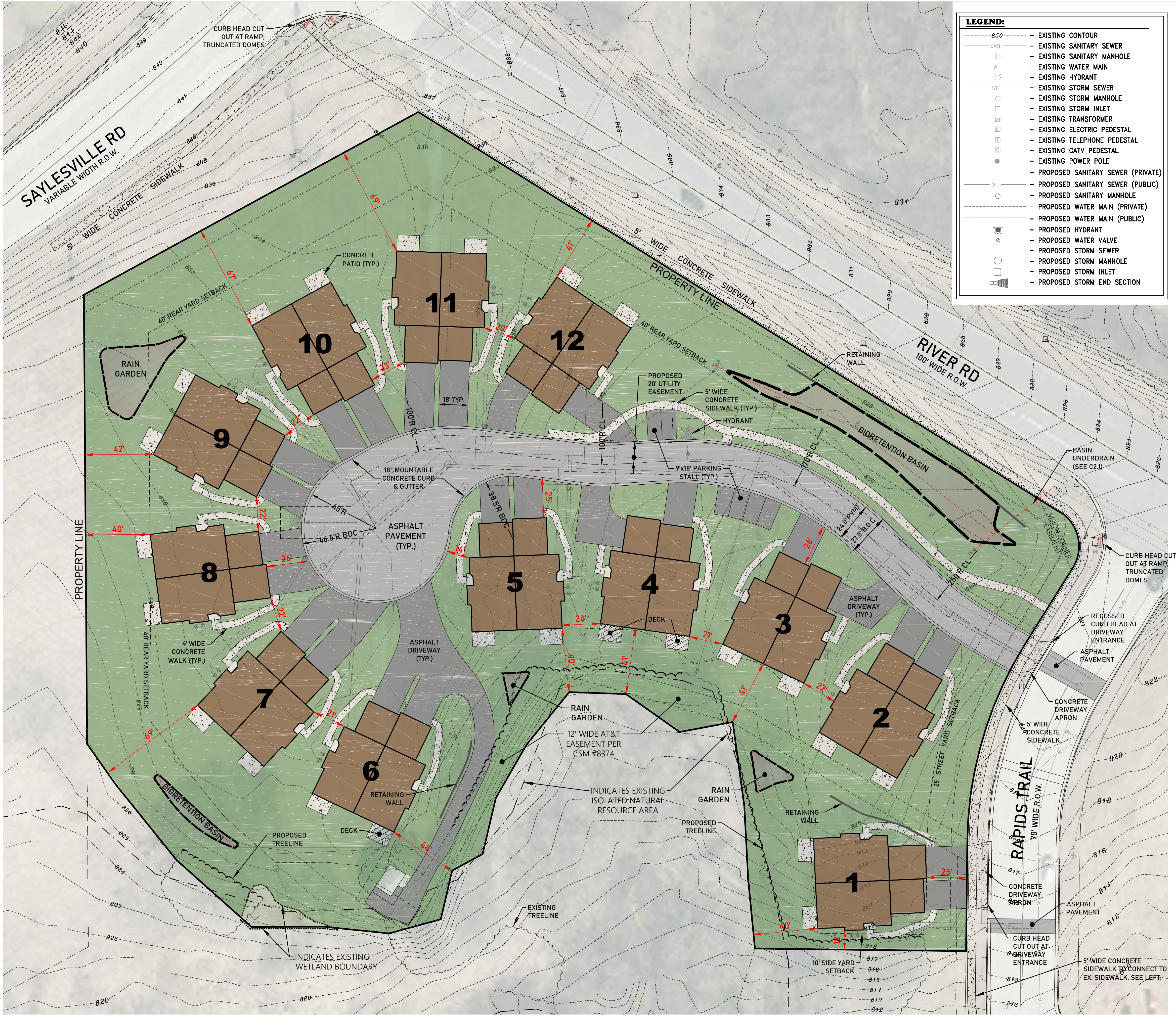
5' WIDE SIDEWALK EXTENSION
SCALE: 1" = 20'



0 15 30 60
SCALE: 1" = 30' (22"x34")
SCALE: 1" = 60' (11"x17")

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PROJECT:
RIVER ROAD VILLAS
CITY OF WAUKESHA, WISCONSIN
BY: BIELINSKI HOMES
1830 MEADOW LN., SUITE A
PEWAUKEE, WI 53072

REVISION HISTORY	
DATE	DESCRIPTION
05/12/25	PRELIMINARY PLANS
08/19/25	ROAD PROFILE REVISION
11/14/25	PER CITY COMMENTS

DATE:
NOVEMBER 14, 2025

JOB NUMBER:
25-09-966

DESCRIPTION:
PROPOSED
SITE PLAN

SHEET

C1.1

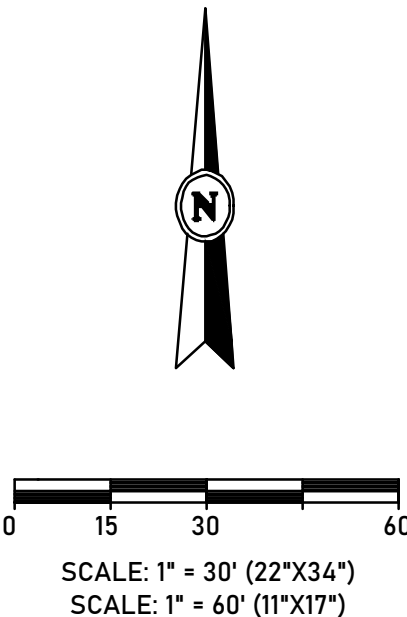
GRADING PLAN NOTES:
1. IN ALL LOCATIONS WHERE ELEVATIONS ARE SHOWN AS ±, THE ELEVATION HAS BEEN DETERMINED USING INTERPOLATED EXISTING SURVEY GRADES. CONTRACTOR SHALL VERIFY ALL EXISTING MATCH GRADES PRIOR TO CONSTRUCTION OF ANY IMPROVEMENTS WITHIN THE PROXIMITY OF THESE INTERPOLATED GRADES. IF DISCREPANCIES ARE PRESENT CONTRACTOR SHALL REPORT THEM TO THE DESIGN ENGINEER FOR VERIFICATION OF PROPOSED SLOPES PRIOR TO INSTALLATION OF PROPOSED IMPROVEMENTS. DESIGN ENGINEER IS NOT RESPONSIBLE FOR SLOPES OF PROPOSED IMPROVEMENTS BASED ON THESE ± GRADES WITHOUT CONFIRMATION OF EXISTING ELEVATIONS AT TIME OF CONSTRUCTION.
2. OVERALL SITE MUST BE GRADED TO WITHIN 4" OF FINAL GRADE PRIOR TO THE ELECTRICAL INSTALLATION.

FFE 100.0 FINISHED FLOOR ELEV.
GAR 100.0 GARAGE ELEV. (AT OVERHEAD DOOR)
FYG 100.0 FRONT YARD GRADE
RYG 100.0 REAR YARD GRADE

TYPICAL BUILDING ELEVATIONS
NOT TO SCALE

GRADING PLAN LEGEND:

FINISHED FLOOR = 100.00
TOP OF FOUNDATION (FRONT, 9' WALL) = 99.82
GARAGE (AT OVERHEAD DOOR) = 99.50
FRONT YARD GRADE = 99.16
TOP OF FOUNDATION (REAR, 8' WALL) = 98.82
REAR YARD GRADE = 98.16
BASEMENT FLOOR = 91.07
TOP OF FOOTING = 90.82



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LEGEND:

- 850 - EXISTING CONTOUR
- 850.0 - PROPOSED CONTOUR
- 850.0 - PROPOSED SPOT ELEVATION
- 850.0 - PROPOSED FLOW ARROW
- FFE 850.0 - PROPOSED YARD GRADE
- GAR 850.0 - PROPOSED GARAGE GRADE
- FYG 850.0 - FINISHED FLOOR ELEVATION
- RYG 850.0 - REAR YARD GRADE
- 850.0 - PROPOSED OVERLAND FLOW ROUTE



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RIVER ROAD VILLAS
CITY OF WAUKESHA, WISCONSIN
BY: BIELINSKI HOMES
1830 MEADOW LN., SUITE A
PEWAUKEE, WI 53072

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08/19/25	ROAD PROFILE REVISION
11/14/25	PER CITY COMMENTS

DATE:
NOVEMBER 14, 2025

JOB NUMBER:
25-09-966

DESCRIPTION:
GRADING &
DRAINAGE PLAN

SHEET

C2.0



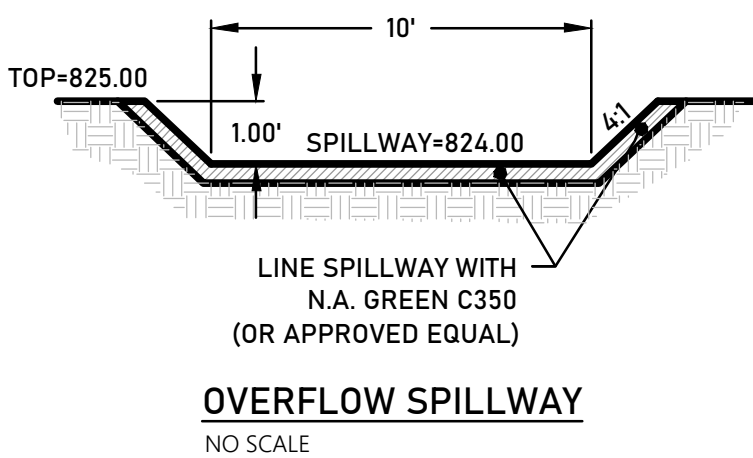
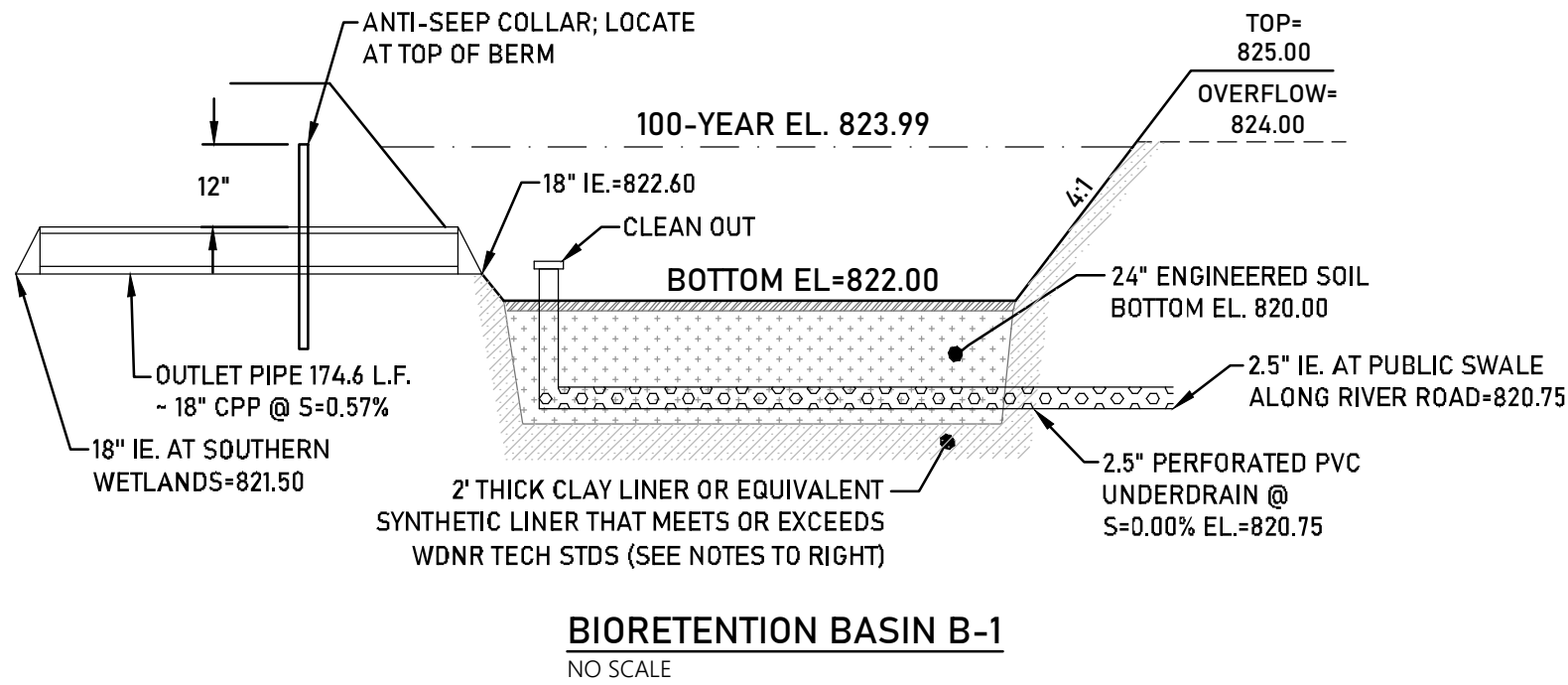
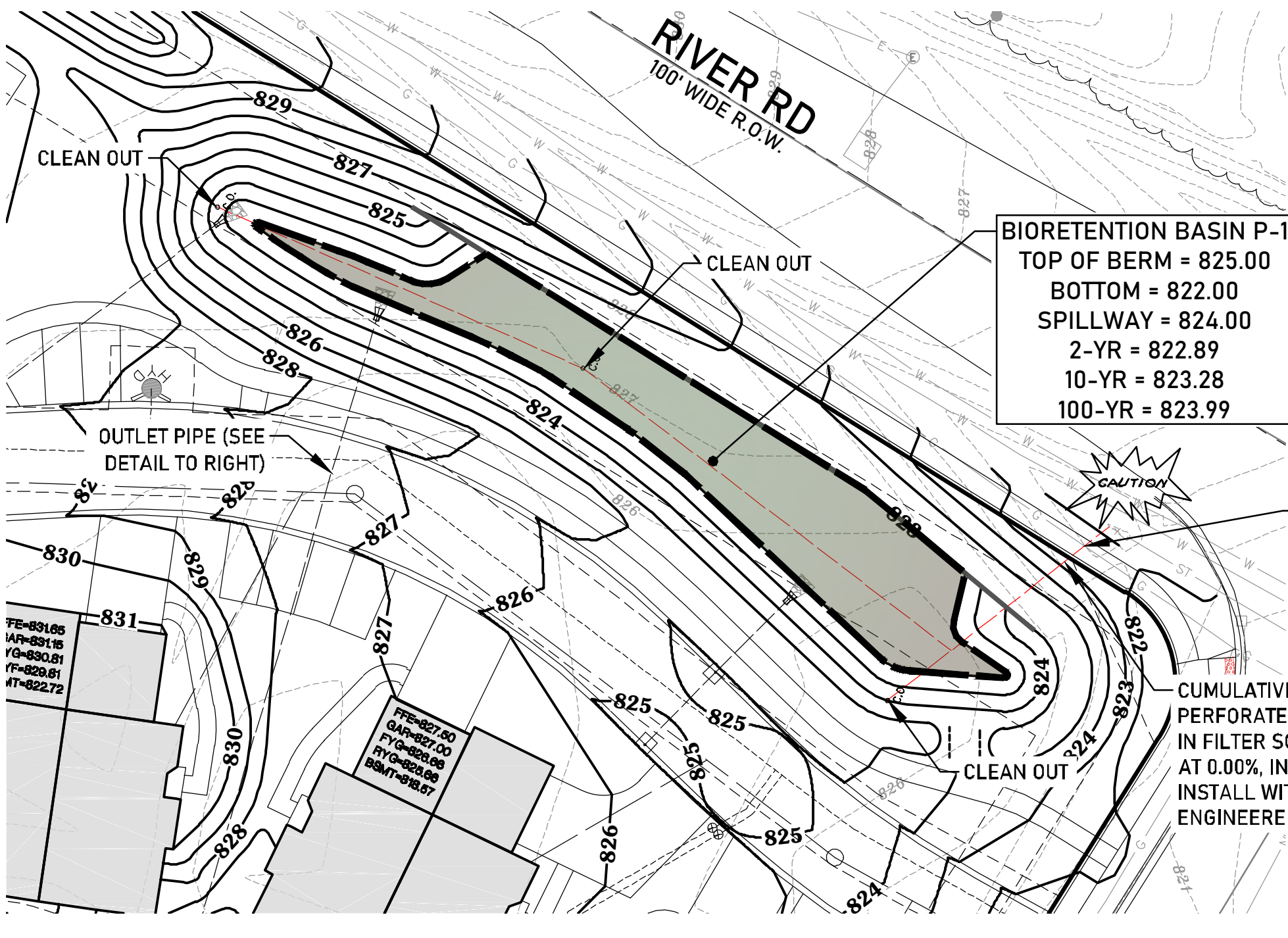
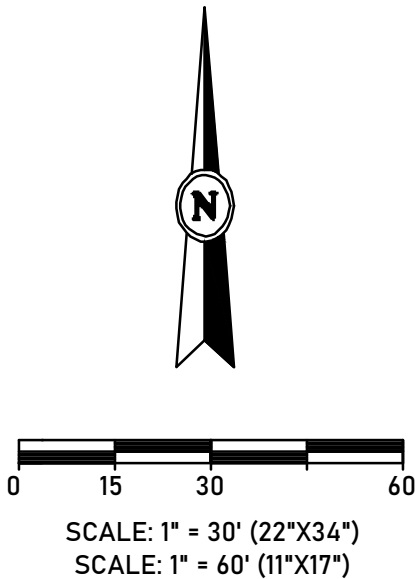
CONTRACTOR IS REQUIRED TO CONTACT DIGGERS HOTLINE TOLL FREE TO OBTAIN LOCATION OF UNDERGROUND UTILITIES PRIOR TO COMMENCING THE WORK. WISCONSIN STATUTE 182.0715 REQUIRES MIN. OF 3 WORK DAYS NOTICE BEFORE YOU EXCAVATE. KNOW WHAT'S BELOW. **CALL BEFORE YOU DIG.**

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LEGEND:

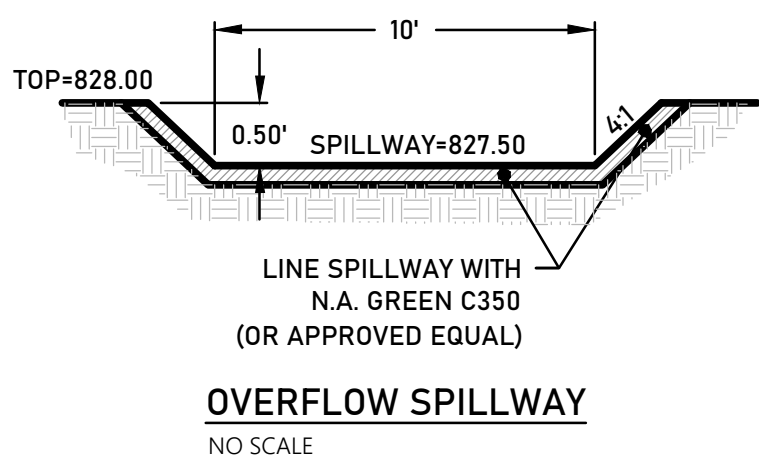
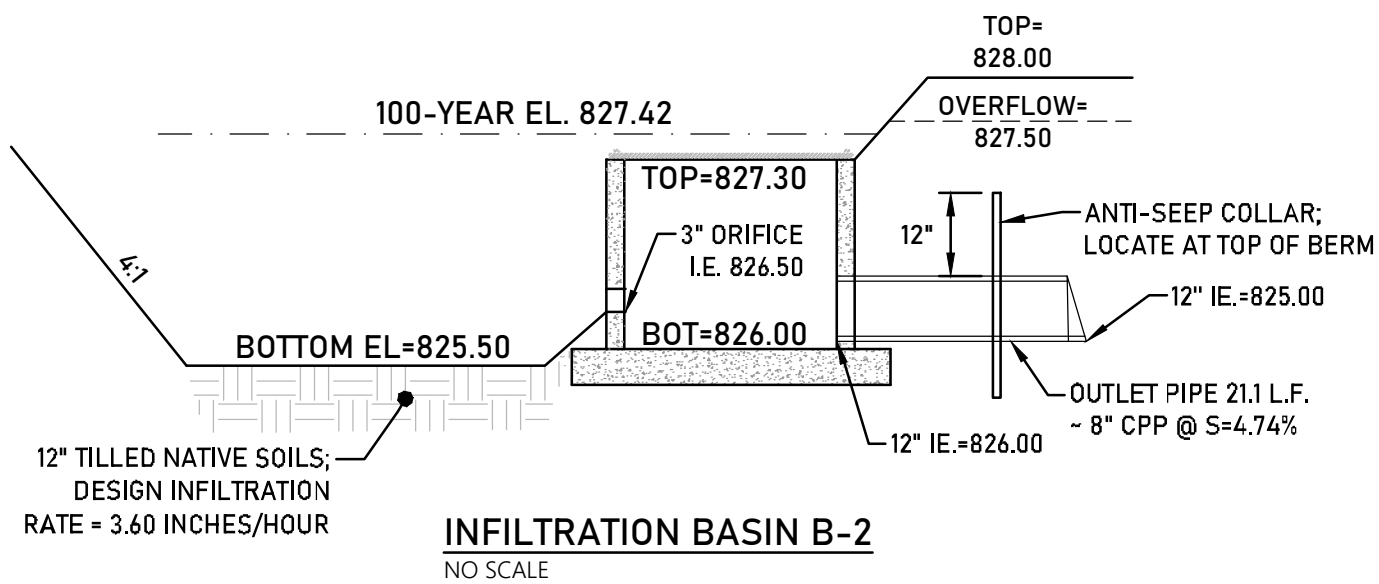
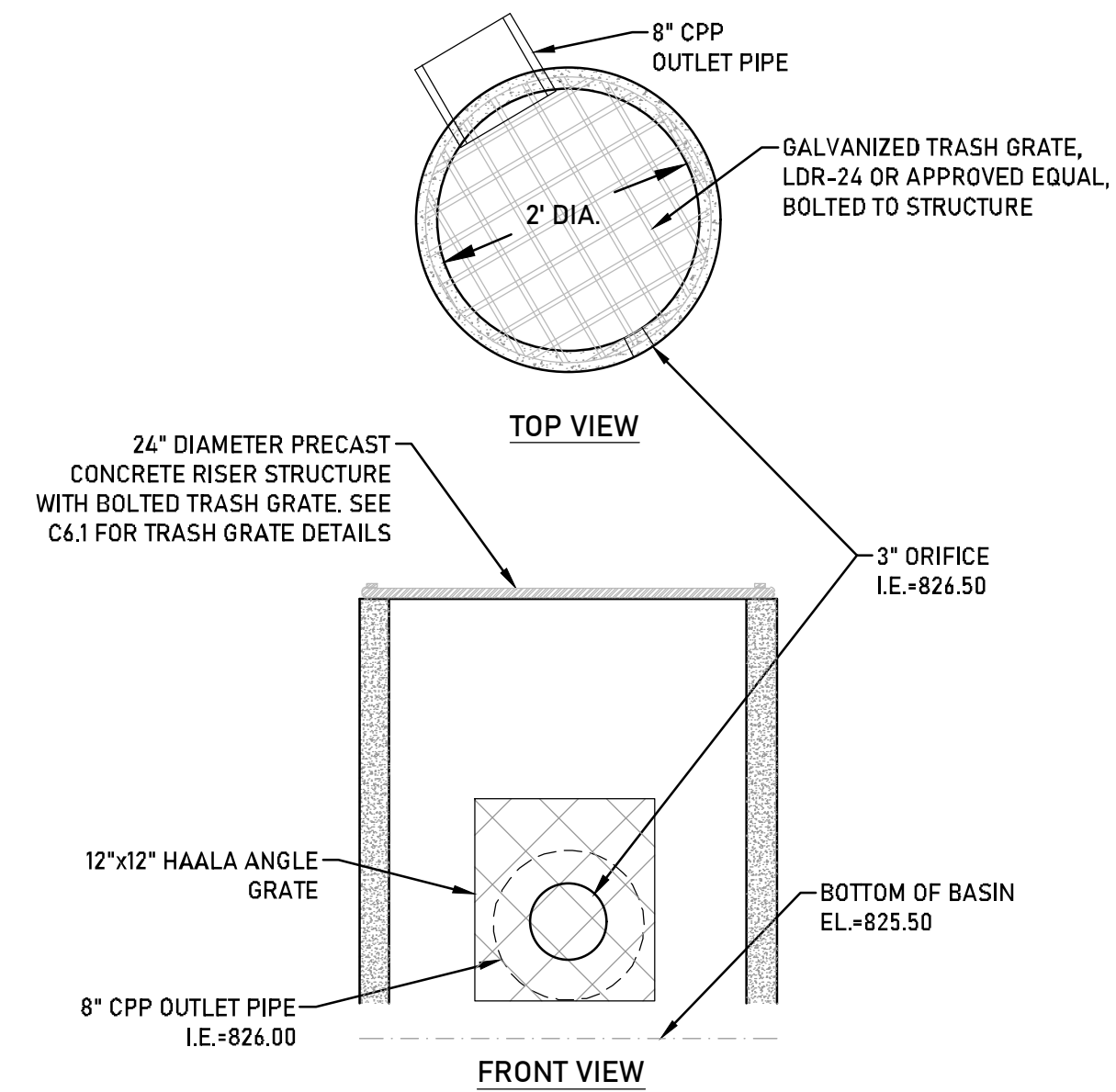
- 850 - EXISTING CONTOUR
- PROPOSED CONTOUR
- PROPOSED SPOT ELEVATION
- PROPOSED FLOW ARROW
- PROPOSED YARD GRADE
- PROPOSED GARAGE GRADE
- FINISHED FLOOR ELEVATION
- REAR YARD GRADE
- PROPOSED OVERLAND FLOW ROUTE

FFE 850.0
GAR 850.0
FYG 850.0
RYG 850.0

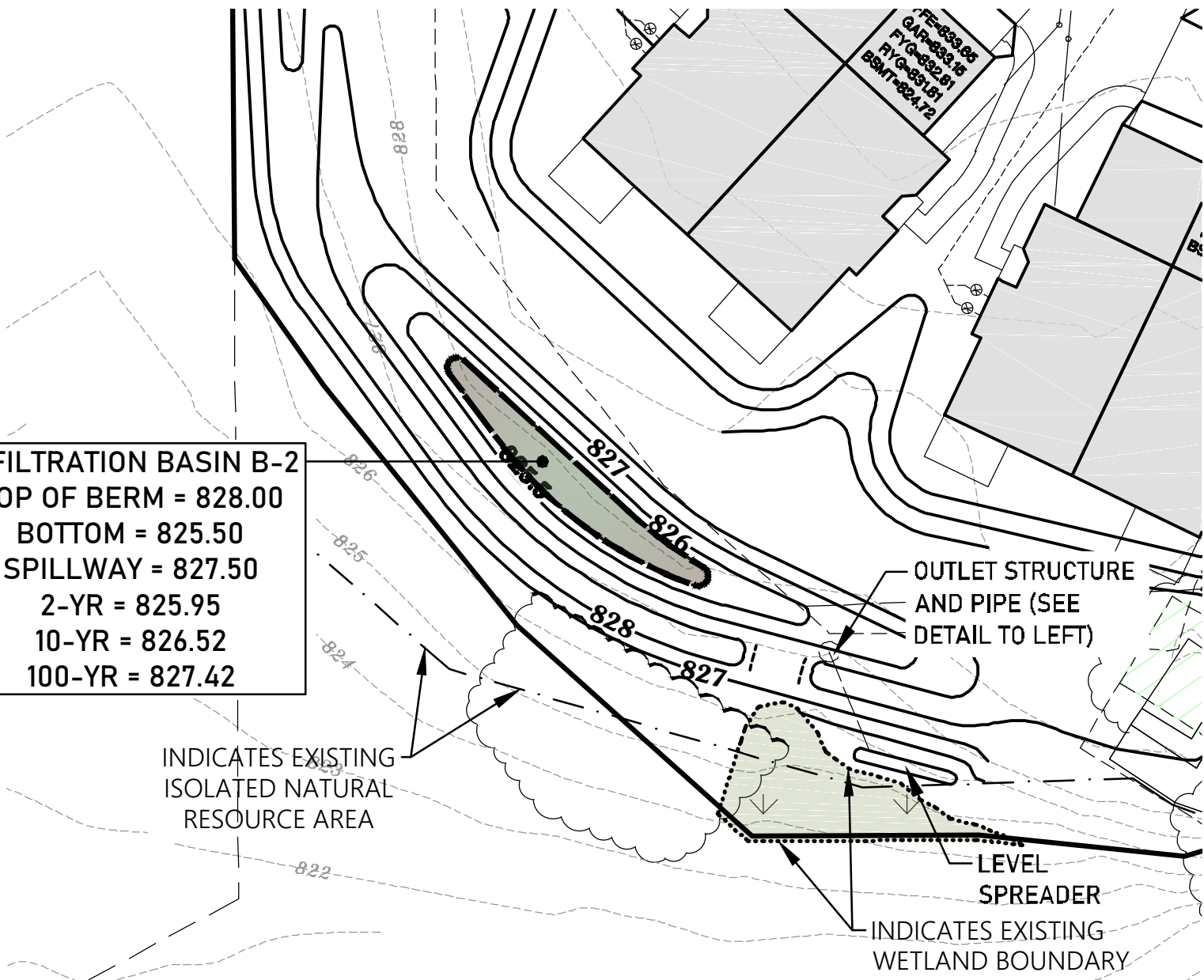


BIORETENTION BASIN B-1

CONTRACTOR TO VERIFY GAS MAIN ELEVATION AND REPORT INVERT CONFLICTS AT UNDERDRAIN CROSSING TO DESIGN ENGINEER.



INFILTRATION BASIN B-2
TOP OF BERM = 828.00
BOTTOM = 825.50
SPILLWAY = 827.50
2-YR = 825.95
10-YR = 826.52
100-YR = 827.42



INFILTRATION BASIN B-2

BIORETENTION BASIN NOTES

ENGINEERED SOIL COMPOSITION:
PLANTING MIXTURE SHALL CONSIST OF A MIXTURE OF 70 TO 85% SAND AND 15 TO 30% COMPOST IN ACCORDANCE WITH WDNR TECHNICAL STANDARD 1004, BIORETENTION FOR INFILTRATION. THE PERCENTAGES ARE BASED ON VOLUME. SPECIAL ATTENTION SHOULD BE GIVEN TO PLANT SELECTION WHEN THE PERCENTAGE OF SAND EXCEEDS 75%.

THE SAND COMPONENT SHALL BE USDA COARSE SAND (0.02 TO 0.04 INCH DIAMETER), PRE-WASHED TO REMOVE CLAY AND SILT PARTICLES, AND WELL-DRAINED OR DRY PRIOR TO MIXING. SAND CONSISTING OF DOLOMITE OR CALCIUM CARBONATE MAY ALSO BE USED.

THE COMPOST COMPONENT SHALL MEET THE REQUIREMENTS OF WDNR SPECIFICATION S100, COMPOST.

BIO-RETENTION PLANTINGS:
SIDE SLOPES OF THE BIO-RETENTION DEVICES SHALL BE PLANTED WITH DOT #20 SEED MIX AND THE BOTTOM OF THE BIO-RETENTION DEVICES SHALL BE PLANTED WITH DOT #70A SEED MIX OR APPROVED EQUIVALENT.

STORAGE LAYER:
THE GRAVEL SHALL MEET THE COARSE AGGREGATE #2 AND OTHER SPECIFICATIONS OF WISCONSIN STANDARDS AND SPECIFICATIONS FOR HIGHWAY AND STRUCTURE CONSTRUCTION, SECTION 501.2.5, 2003 EDITION, OR AN EQUIVALENT AS APPROVED BY THE ADMINISTERING AUTHORITY. GRAVEL SHALL BE DOUBLE-WASHED.

A LAYER OF SAND MAY BE USED IN LIEU OF GRAVEL TO FORM THE STORAGE LAYER. SEE SAND SPECIFICATION NOTED UNDER ENGINEERED SOIL COMPOSITION.

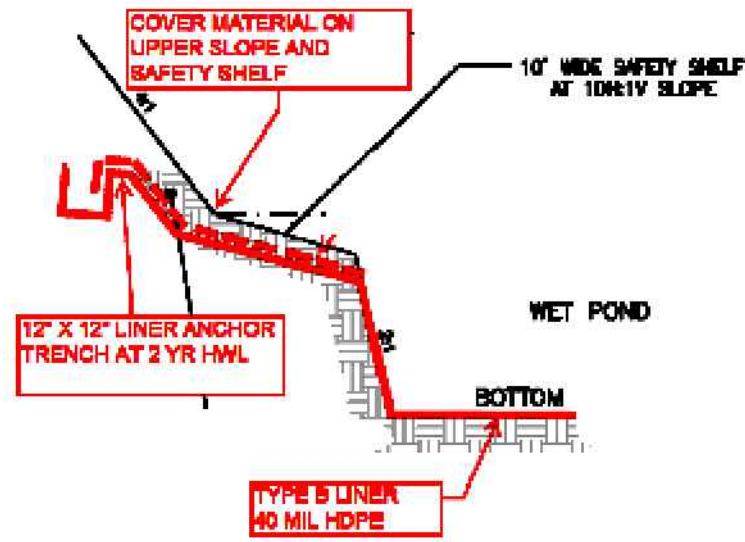
UNDERDRAIN PIPE PROTECTION:
THE UNDERDRAIN PIPE SHALL BE PROTECTED FROM CLOGGING BY USE OF FILTER FABRIC OR A FILTER SOCK. IF THE STORAGE LAYER IS SAND, A FILTER SOCK SHALL BE USED. A COVER OF PEA GRAVEL MAY ALSO BE USED.

FILTER FABRIC - FILTER FABRIC SHALL COVER THE UNDERDRAIN PIPE AND SHALL NOT EXTEND LATERALLY FROM EITHER SIDE OF THE PIPE MORE THAN TWO FEET. THE FABRIC SHALL MEET THE SPECIFICATIONS OF WISCONSIN STANDARDS AND SPECIFICATIONS FOR HIGHWAY AND STRUCTURE CONSTRUCTION, SECTION 645.2.4, SCHEDULE TEST B, 2003 EDITION, OR AN EQUIVALENT APPROVED BY THE ADMINISTERING AUTHORITY.

FILTER SOCK - THE OPENINGS IN THE FILTER SHALL BE SMALL ENOUGH TO PREVENT SAND PARTICLES FROM ENTERING THE UNDERDRAIN PIPE. THE FLOW RATE OF THE FABRIC SHALL BE CAPABLE OF PASSING WATER AT A RATE EQUAL TO OR GREATER THAN THE FLOW RATE CAPACITY OF THE TOTAL COMBINED PERFORATIONS IN THE UNDERDRAIN PIPE. IN ADDITION, THE FABRIC SHALL MEET THE OTHER REQUIREMENTS OF WISCONSIN STANDARDS AND SPECIFICATIONS FOR HIGHWAY AND STRUCTURE CONSTRUCTION, SECTION 612.2.8(1-3), 2003 EDITION, OR AN EQUIVALENT APPROVED BY THE ADMINISTERING AUTHORITY.

CLAY LINER NOTE:
- 2" THICK CLAY LINER IN ACCORDANCE WITH WDNR TECH. STD. 1001 TYPE B, AND SOILS REPORT.
- CLAY LINER MATERIALS SHOULD BE COMPACTED TO A MINIMUM OF 90 TO 92% DRY DENSITY AS DETERMINED BY MODIFIED PROCTOR (ASTM D1557)
- IF PREDOMINANTLY GRANULAR TEXTURE SOILS AND/OR GROUNDWATER INFLOW IS ENCOUNTERED AT OR BELOW THE BOTTOM ELEVATION OF THE POND, A "SACRIFICIAL" LIFT OF CLAYEY FILL MAY BE REQUIRED TO BE PLACED ALONG THE SIDEWALLS AND BASE PRIOR TO COMMENCING WITH ACTUAL LINER PLACEMENT/CONSTRUCTION TO AID IN ATTAINING A SATISFACTORILY COMPACTED LINER SECTION. THE PLACEMENT OF A "SACRIFICIAL" LAYER IS PARTICULARLY BENEFICIAL IN CREATING A STABLE SUBBASE IN THE EVENT SLIGHT GROUNDWATER SEEPAGE OCCURS.
- GEOTECHNICAL TESTING SHALL BE PERFORMED ON POTENTIAL CLAY LINER MATERIAL TO CONFIRM THAT SPECIFICATIONS ARE MET.
- CLAY LINER SHALL EXTEND UP TO 100-YEAR WATER SURFACE ELEVATION.

HDPE LINER NOTE:
- TYPE B HDPE LINER
- LINER PRODUCT INFORMATION:
- MIN. THICKNESS = 40 MILS
- COLOR = BLACK
- SUPPLIER = SOLMAX
- INSTALLED PER MANUFACTURER SPECIFICATIONS
- LINER SHALL BE TRENCHED INTO POND SLOPE PER DETAIL AND SAFETY SHELF COVERED WITH MIN. 6" OF TOPSOIL.



TWO-PIECE ANTI-SEEP COLLAR NOTES:
- ANTI-SEEP COLLAR MATERIAL SHALL MATCH OUTLET PIPE MATERIAL.
- ANTI-SEEP COLLARS SHALL BE 1/4" POLYETHYLENE SHEETS WITH COLLAR FOR PIPE. DIMENSIONS SHALL BE AS NOTED ON THE DETAIL.
- INSTALL ANTI-SEEP COLLARS PER MANUFACTURER SPECIFICATIONS.



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PROJECT:
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CITY OF WAUKESHA, WISCONSIN
BY: BIELINSKI HOMES
1830 MEADOW LN., SUITE A
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REVISION HISTORY

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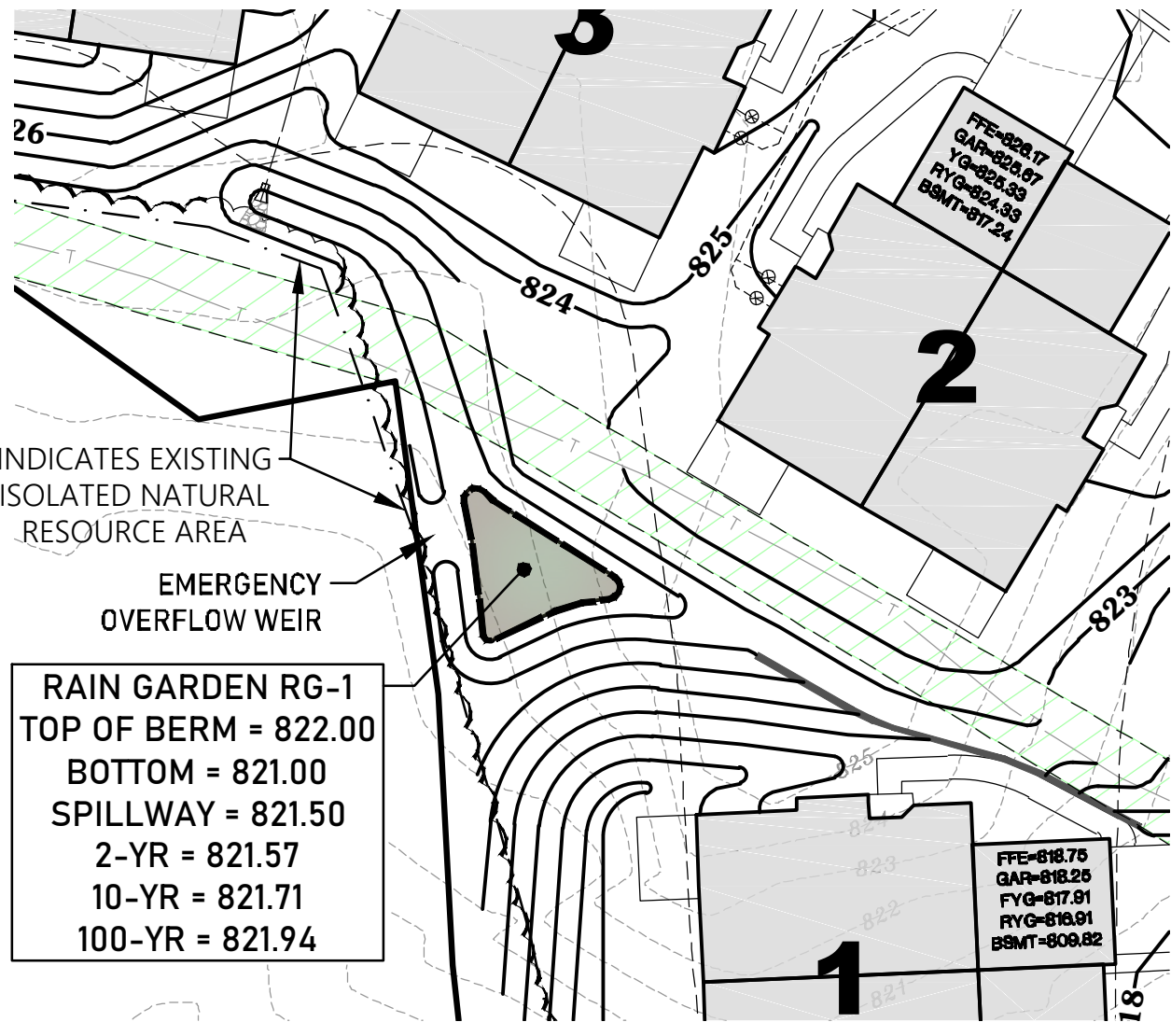
DESCRIPTION:

STORMWATER
FACILITY DETAILS

SHEET

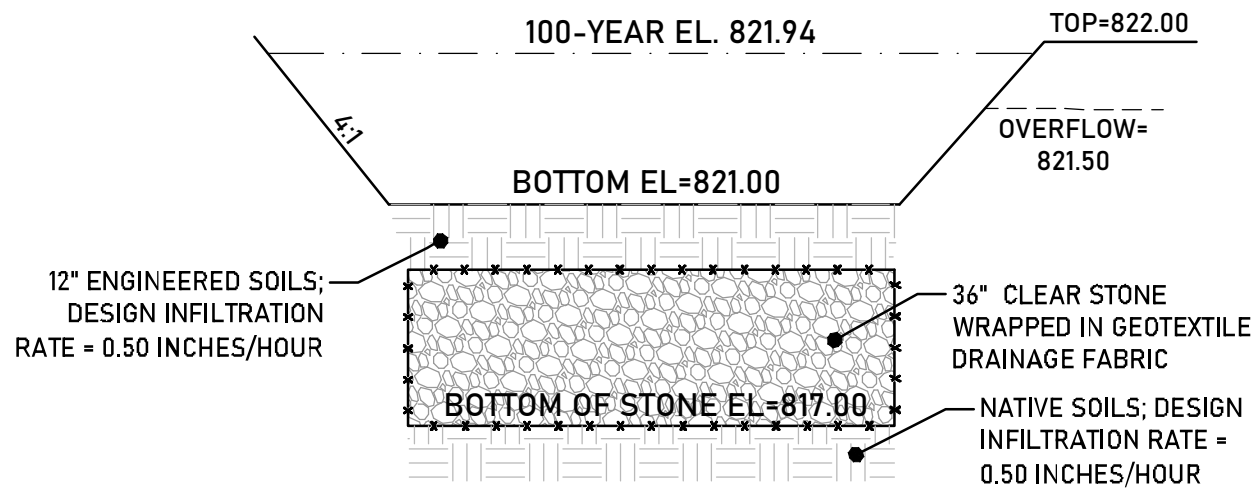
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X:\2025\25-09-966 RIVER ROAD VILLAS - WAUKESHA\DRAWINGS\CONSTRUCTION PLANS\RIVER ROAD VILLAS_CIVIL PLAN_SET_22X34.DWG

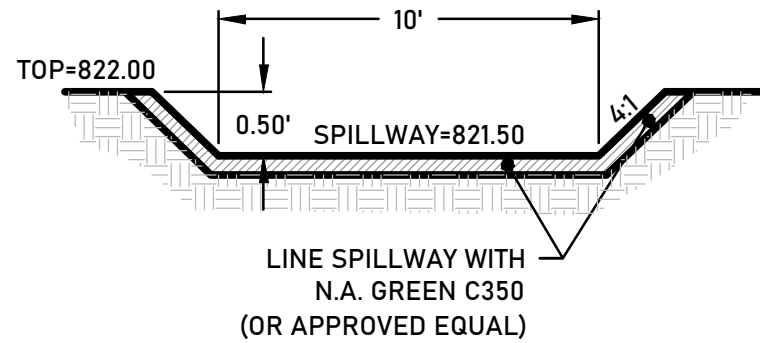


RAIN GARDEN RG-1
TOP OF BERM = 822.00
BOTTOM = 821.00
SPILLWAY = 821.50
2-YR = 821.57
10-YR = 821.71
100-YR = 821.94

RAIN GARDEN RG-1



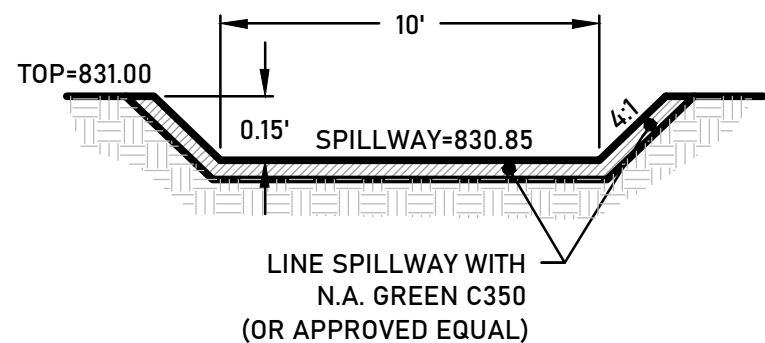
RAIN GARDEN RG-1
NO SCALE



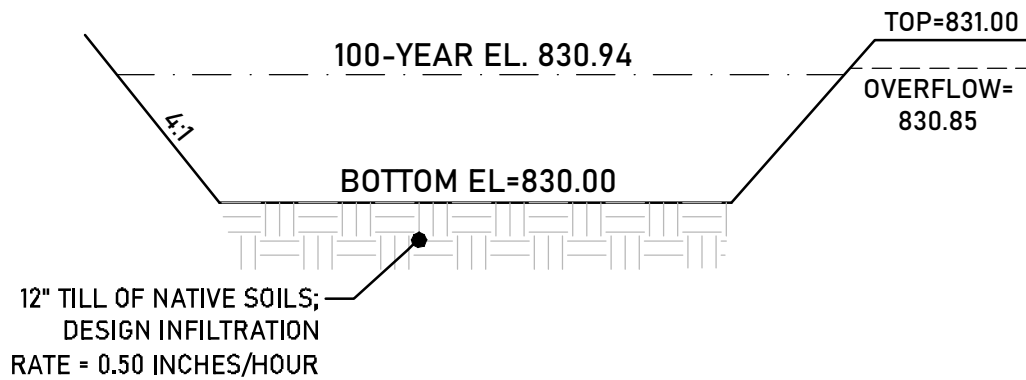
OVERFLOW SPILLWAY RG-1
NO SCALE

RAIN GARDEN NOTES

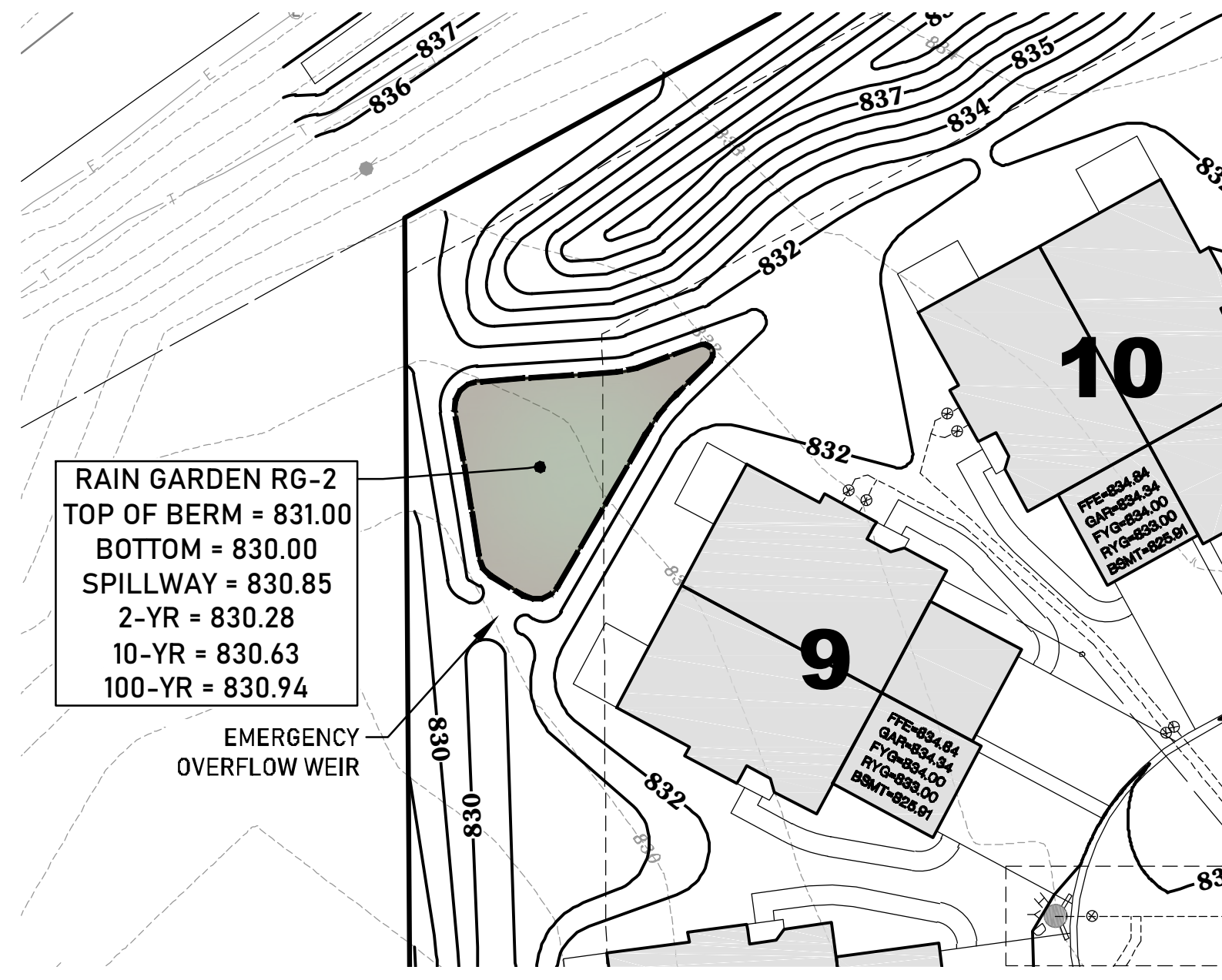
1. FINAL RAIN GARDEN WORK SHALL NOT BE INSTALLED UNTIL THE SITE IS SUBSTANTIALLY STABILIZED.
2. DURING ALL PHASES OF CONSTRUCTION, THE BASIN AREAS SHALL BE PROTECTED FROM HEAVY EQUIPMENT COMPACTION.
3. REMOVE ACCUMULATED SEDIMENT PRIOR TO TILLING OF NATIVE SOILS.
4. TILLED NATIVE SOILS IN THE RAIN GARDEN AREAS SHALL NOT BE COMPACTED.



OVERFLOW SPILLWAY RG-2
NO SCALE

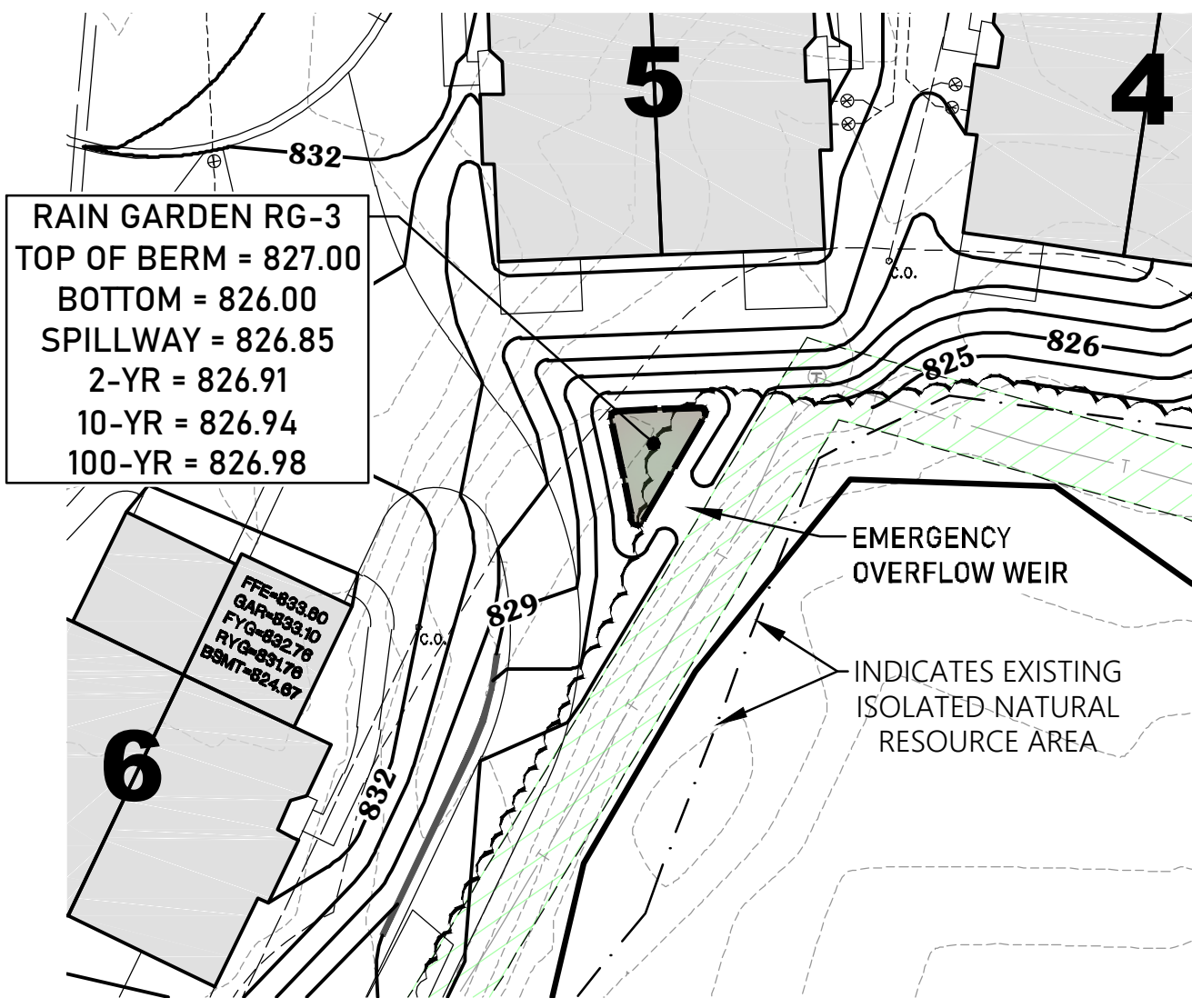


RAIN GARDEN RG-2
NO SCALE

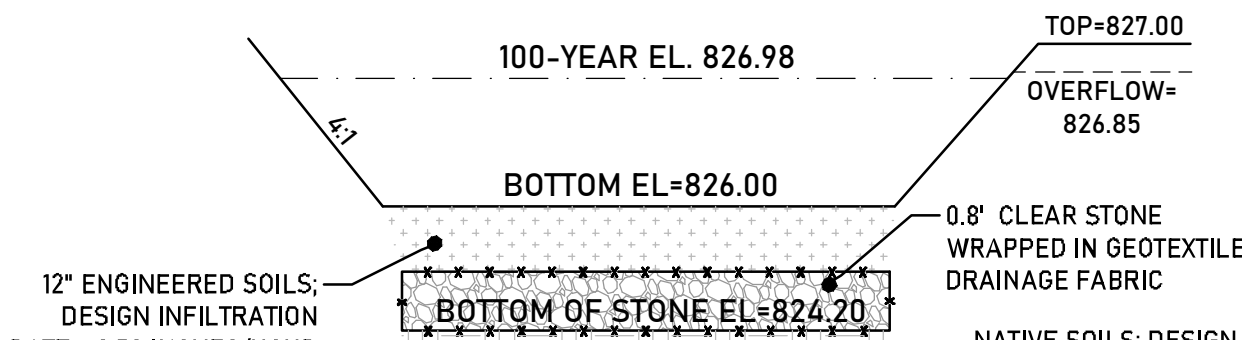


RAIN GARDEN RG-2
TOP OF BERM = 831.00
BOTTOM = 830.00
SPILLWAY = 830.85
2-YR = 830.28
10-YR = 830.63
100-YR = 830.94

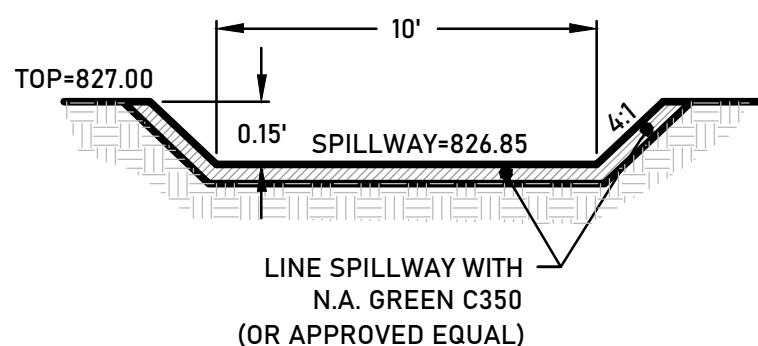
RAIN GARDEN RG-2



RAIN GARDEN RG-3
TOP OF BERM = 827.00
BOTTOM = 826.00
SPILLWAY = 826.85
2-YR = 826.91
10-YR = 826.94
100-YR = 826.98



RAIN GARDEN RG-3
NO SCALE



OVERFLOW SPILLWAY RG-3
NO SCALE



0 15 30 60
SCALE: 1" = 30' (22"x34")
SCALE: 1" = 60' (11"x17")

LEGEND:	
	- EXISTING CONTOUR
	- PROPOSED CONTOUR
	- PROPOSED SPOT ELEVATION
	- PROPOSED FLOW ARROW
	- PROPOSED YARD GRADE
	- PROPOSED GARAGE GRADE
	- FINISHED FLOOR ELEVATION
	- REAR YARD GRADE
	- PROPOSED OVERLAND FLOW ROUTE



CONTRACTOR IS REQUIRED TO CONTACT DIGGERS HOTLINE TOLL FREE TO OBTAIN LOCATION OF UNDERGROUND UTILITIES PRIOR TO COMMENCING THE WORK. WISCONSIN STATUTE 182.0715 REQUIRES MIN. OF 3 WORK DAYS NOTICE BEFORE YOU EXCAVATE. KNOW WHAT'S BELOW. **CALL BEFORE YOU DIG.**

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BROOKFIELD, WI 53045
PHONE: (262) 790-1480
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PROJECT:
RIVER ROAD VILLAS
CITY OF WAUKESHA, WISCONSIN
BY: BIELINSKI HOMES
1830 MEADOW LN., SUITE A
PEWAUKEE, WI 53072

REVISION HISTORY

DATE	DESCRIPTION
05/12/25	PRELIMINARY PLANS
08/19/25	ROAD PROFILE REVISION
11/14/25	PER CITY COMMENTS

DATE:

NOVEMBER 14, 2025

JOB NUMBER:

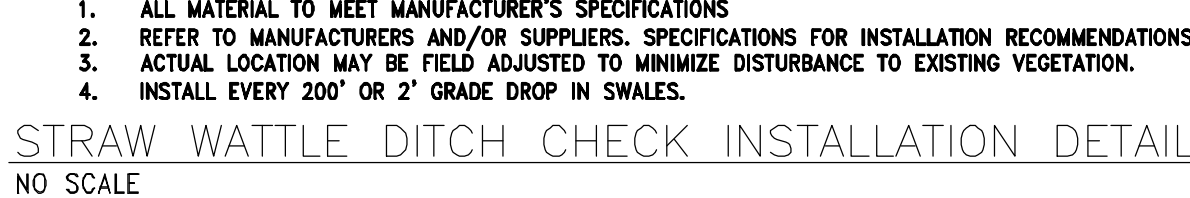
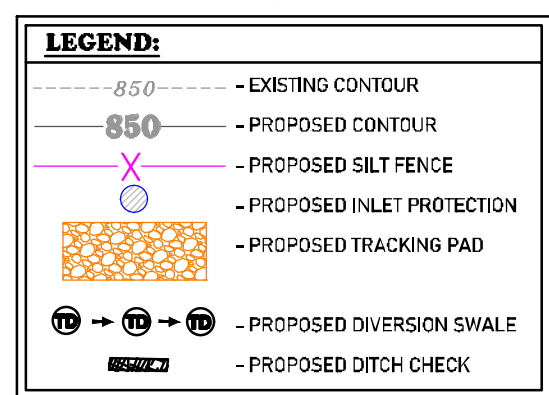
25-09-966

DESCRIPTION:

STORMWATER
FACILITY DETAILS

SHEET

C2.2

[illegible]

GEOTEXTILE FABRIC, TYPE FF

2"x4" STAKE AND CROSS BRACING

DIRECTION OF RUNOFF WATER FLOW

GRATED INLET

BURIED FABRIC, MIN. 6" DEPTH

GEOTEXTILE FABRIC, TYPE FF

2"x4" STAKE AND CROSS BRACING

2'

INLET WITH OR WITHOUT GRATE

ATTACH GEOTEXTILE FABRIC, TYPE FF TO THE STAKES AND CROSS BRACING

INLET PROTECTION, TYPE A

GEOTEXTILE FABRIC, TYPE FF

FRONT, BACK, AND BOTTOM TO BE MADE FROM SINGLE PIECE OF FABRIC.

MINIMUM DOUBLE-STITCHED SEAMS ALL AROUND SIDE PEGS AND ON FLAP POCKETS.

1

GEOTEXTILE FABRIC, TYPE FF

INLET PROTECTION, TYPE B (WITHOUT CURB BOX)

(CAN BE INSTALLED IN ANY INLET WITHOUT A CURB BOX)

GEOTEXTILE FABRIC, TYPE FF

INLET PROTECTION, TYPE C (WITH CURB BOX)

INLET SPECIFICATIONS AS PER THE PLAN DIMENSION LENGTH AND WIDTH TO MATCH.

FLAP POCKET

3

1

2

3

4"

12"

17"

USE REBAR OR STEEL ROD FOR REMOVAL OR FOR INLETS WITH CAST-IN WOOD 2x4. EXTEND 10" BEYOND GRATE WIDTH ON BOTH SIDES. LENGTH VARIES. SECURE TO GRATE WITH WIRE OR PLASTIC TIES.

4"x6" OVAL HOLE SHALL BE HEAT CUT INTO ALL FOUR SIDE PANELS.

INLET PROTECTION, TYPE D (CAN BE INSTALLED IN ANY INLET TYPE WITH OR WITHOUT A CURB BOX AS PER NOTE 2)

INLET PROTECTION DETAIL
NO SCALE

CONSTRUCTION SEQUENCE PLAN

1. SPRING 2026: STAKE WETLAND AND FLOODPLAIN BOUNDARY. INSTALL TRACKING PAD AND SILT FENCE AROUND THE PERIMETER OF THE SITE.
- 1.1. THE WETLAND BOUNDARIES SHOWN ON THE PLANS SHALL BE STAKED PER THE ACTUAL WETLAND FIELD DELINEATION PRIOR TO INSTALLATION OF EROSION CONTROL PERIMETER MEASURES AND SITE DISTURBANCE.
2. SUMMER 2026: CLEAR AND GRUB SITE.
3. SUMMER 2026: STRIP TOPSOIL AND STOCKPILE INITIAL TOPSOIL ALONG NORTH PERIMETER. SEED AND STABILIZE STOCKPILES IMMEDIATELY UPON COMPLETION OF STOCKPILING.
- 3.1. ALL EXPOSED SOIL AREAS NOT DISTURBED FOR UP TO SEVEN (7) DAYS MUST BE IMMEDIATELY RESTORED WITH SEED AND MULCH.
4. SUMMER 2026: INSTALL DIVERSION BERMS WITH DITCH CHECKS AND SEDIMENT TRAPS. DIRECTING SITE RUNOFF TO SEDIMENT TRAPS AS INDICATED ON THE PLAN.
5. SUMMER 2026: ROUGH GRADE SITE.
6. SUMMER 2026: INSTALL SANITARY SEWER AND WATER MAIN. COMPLETE ALL TESTING AND CONTACT CITY PRIOR TO CONNECTING TO EXISTING MAINS. NOTE THAT THIS STEP MAY BEGIN CONCURRENT WITH ROUGH GRADING ONCE THE UTILITY AREAS HAVE BEEN GRADED TO SUBGRADE.
7. SUMMER 2026: INSTALL STORM SEWER, AND ASSOCIATED EROSION CONTROL MEASURES PER EROSION CONTROL PLAN. NOTE THAT THIS STEP MAY BEGIN CONCURRENT WITH SANITARY SEWER AND WATER MAIN INSTALLATION.
8. SUMMER/FALL 2026: FINISH GRADE THE SITE. NOTE THAT THIS STEP MAY OCCUR CONCURRENT WITH STORM SEWER INSTALLATION.
9. SUMMER/FALL 2026: INSTALL STONE BASE COURSE, CONSTRUCT CURB, AND PLACE BINDER COURSE PAVEMENT. NOTE THAT THIS STEP MAY BEGIN CONCURRENT WITH FINISH GRADING.
10. FALL 2026: RESPREAD TOPSOIL AND PLANT SEED, MULCH, MATTING AND OTHER SITE STABILIZERS AS NEEDED. NOTE THAT THIS STEP MAY BEGIN CONCURRENT WITH FINISH GRADING AND ROAD CONSTRUCTION. ALSO NOTE THAT ANY AREAS, SUCH AS POND SLOPES, THAT ARE FINISH GRADED AND TOPSOILED SHALL BE SEEDED, MULCHED AND/OR MATTED IMMEDIATELY AFTER TOPSOIL IS PLACED.
11. REMOVE TEMPORARY SEDIMENT TRAPS AND CONSTRUCT STORMWATER MANAGEMENT BASINS PER PLAN. ENSURE ALL SILT HAS BEEN REMOVED AND NATIVE SOILS HAVE BEEN EXPOSED, PROVIDE 12" DEEP MINIMUM TILL OF NATIVE SOILS PRIOR TO ADDING ENGINEERED SOILS. IF OVER EXCAVATION IS REQUIRED IN THIS STEP, FILL EXCAVATION WITH ON SITE GRANULAR MATERIAL TO ELEVATION NEEDED TO IMPLEMENT BASIN DESIGN.
12. FALL 2026: LATE SEASON (AFTER OCTOBER 15) STABILIZATION NOTES:
 - 12.1. ALL AREAS THAT WILL REMAIN DISTURBED AFTER OCTOBER 15 MUST BE TEMPORARY SEEDED AS OF THAT DATE.
- 12.2. AN ANTIMACROBIAL ACTION THAT OCCURS AFTER OCTOBER 15 MAY REQUIRE CONTRACTOR TO COVER DISTURBED AREAS WITH THE ANIONIC FORM OF POLYACRYLAMIDE (PAM), PER WDNR TECHNICAL STANDARD 1050 AND WisDOT PAL LIST. CONTRACTOR SHALL COORDINATE WITH CITY ENGINEER AND WDNR FOR LATE SEASON STABILIZATION REQUIREMENTS.
13. SPRING/SUMMER 2027: REMOVE CONSTRUCTION EROSION CONTROL MEASURES AFTER SITE IS STABILIZED (SUBSTANTIALLY VEGETATED) AND ALL CONSTRUCTION IS COMPLETE.



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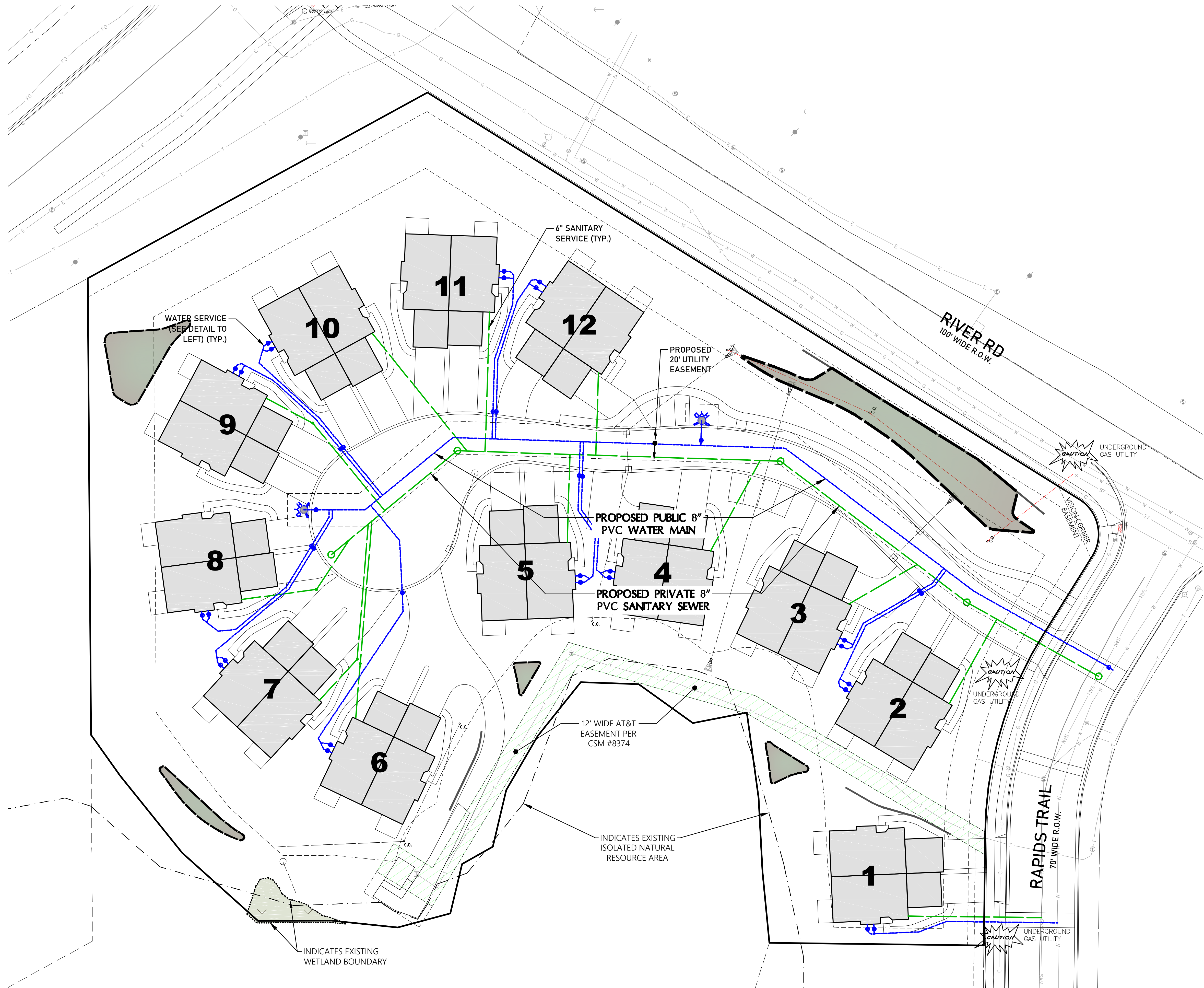
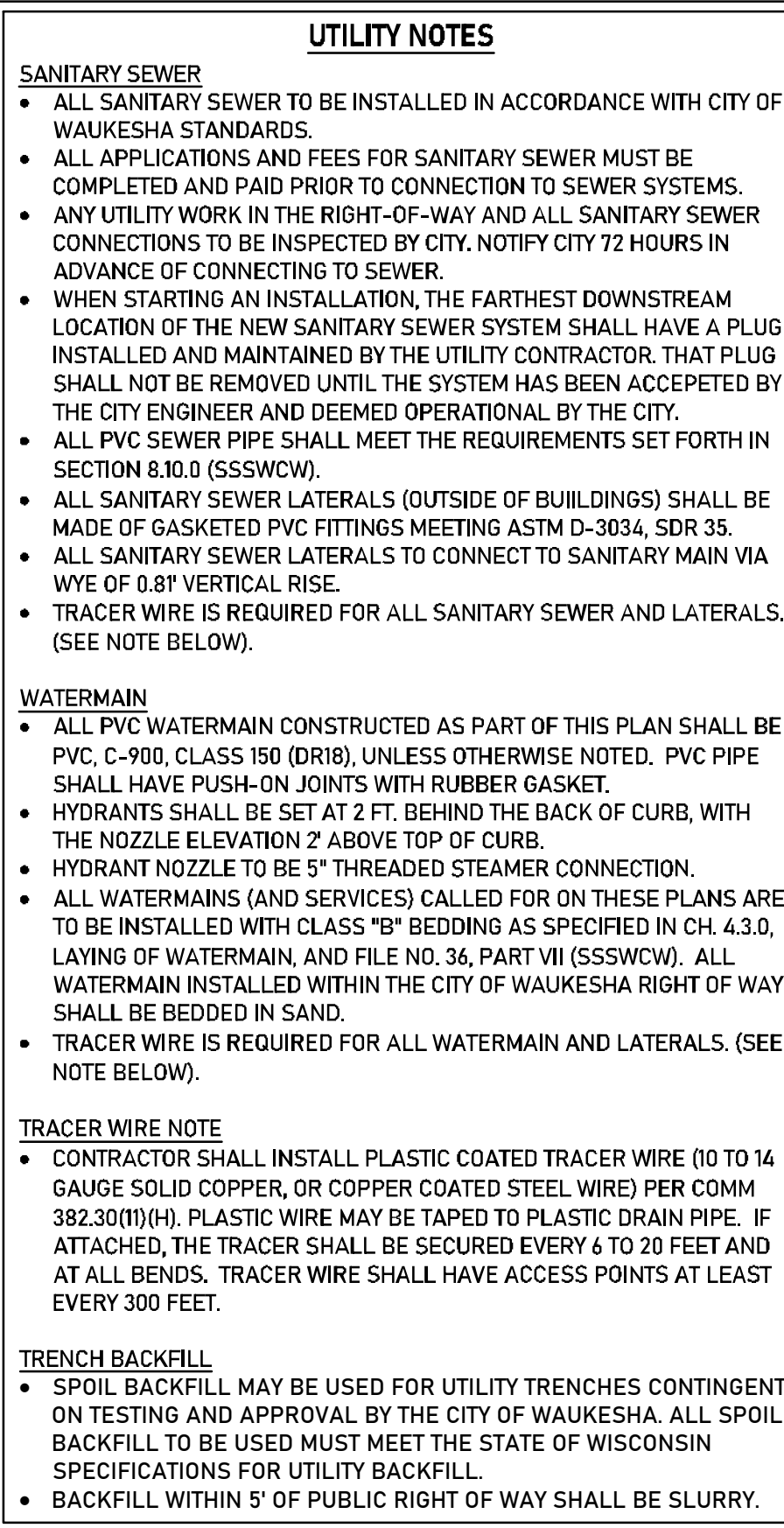
JOB NUMBER:
25-09-966

DESCRIPTION:

EROSION
CONTROL PLAN

SHEET

C3.0



PROJECT:
RIVER ROAD VILLAS
CITY OF WAUKESHA, WISCONSIN
BY: BIELINSKI HOMES
1830 MEADOW LN., SUITE A
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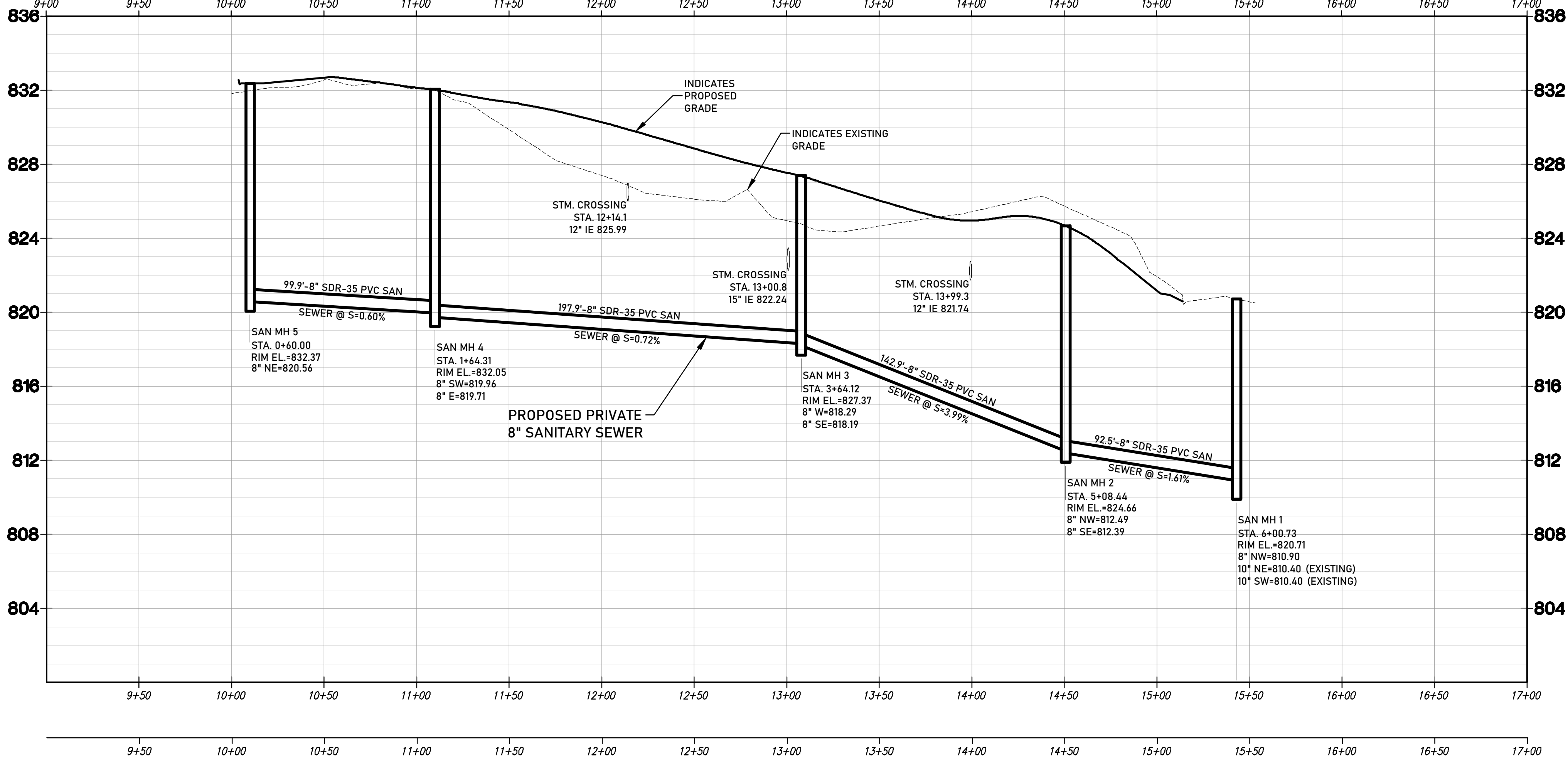
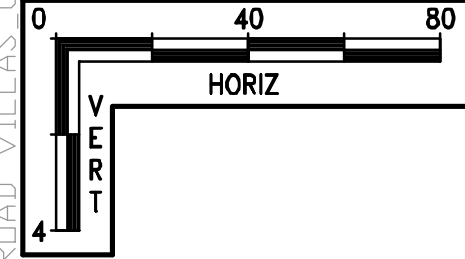
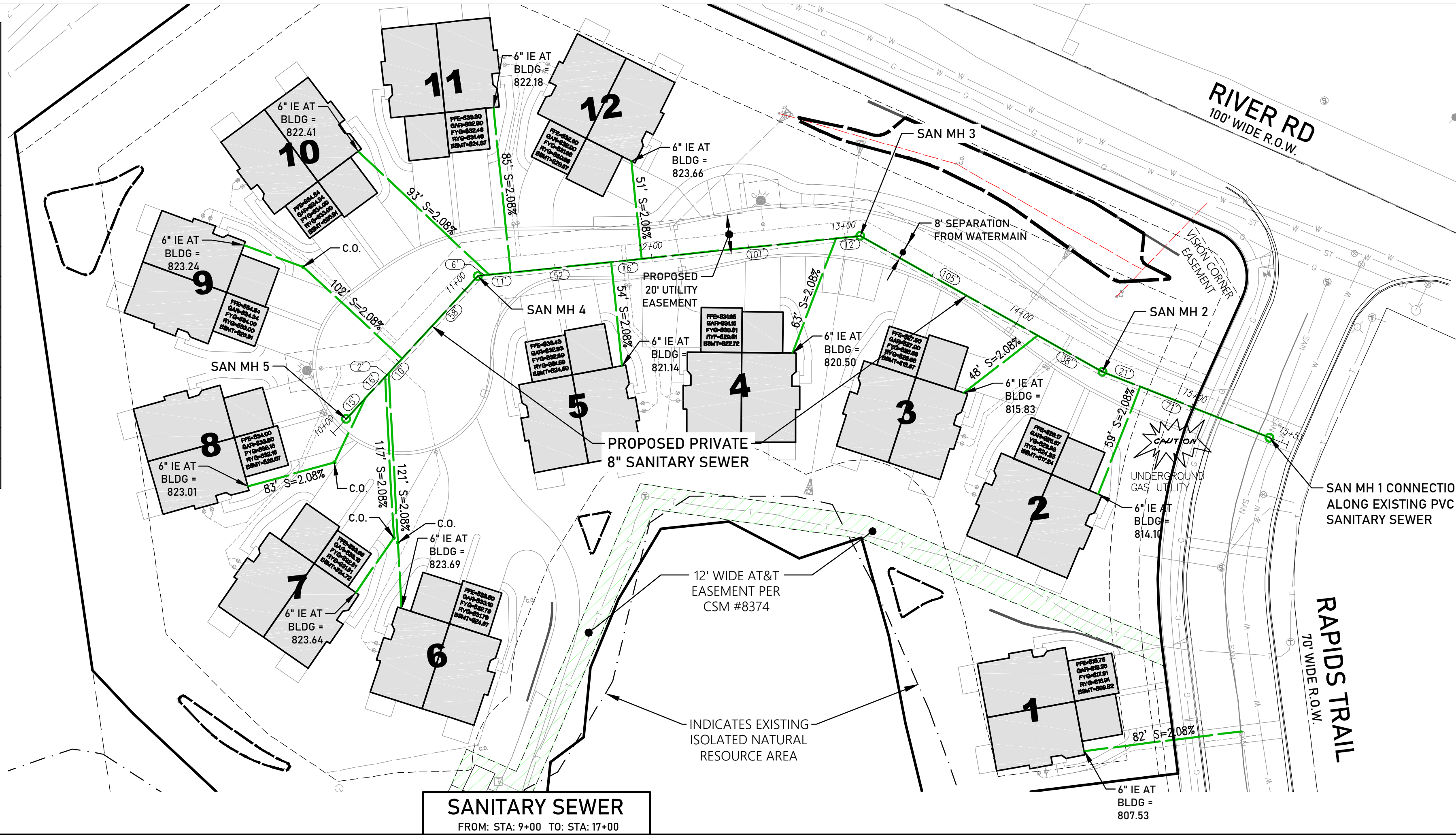
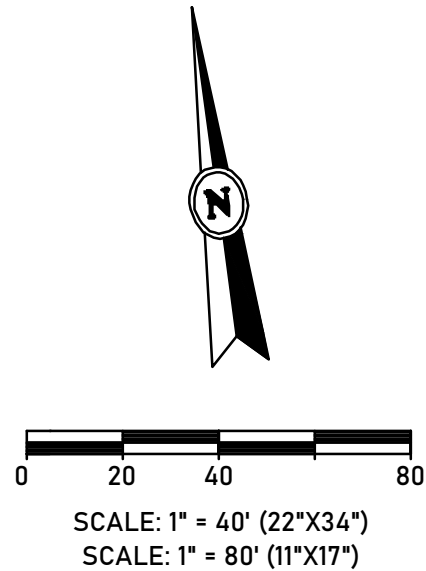
JOB NUMBER:
25-09-966

DESCRIPTION:
OVERALL
SANITARY &
WATER PLAN

SHEET

C4.0

Unit Number	Sewer Main Invert Elevation (I.E.)	Sewer Main Size	Wye Height	6" Lateral Slope	Lateral Length to Pad	6" Lateral I.E. at Pad	Bottom of 9' Foundation Wall and Footing
1	804.88±	10	0.95	2.08%	81.8	807.53	809.91
2	812.05	8	0.81	2.08%	59.3	814.10	817.33
3	814.01	8	0.81	2.08%	48.3	815.83	818.66
4	818.38	8	0.81	2.08%	63.1	820.50	822.81
5	819.21	8	0.81	2.08%	53.7	821.14	824.59
6	820.37	8	0.81	2.08%	120.5	823.69	824.81
7	820.38	8	0.81	2.08%	117.4	823.64	824.81
8	820.47	8	0.81	2.08%	83.0	823.01	825.16
9	820.31	8	0.81	2.08%	101.6	823.24	825.66
10	819.67	8	0.81	2.08%	92.6	822.41	825.66
11	819.59	8	0.81	2.08%	85.4	822.18	824.46
12	819.10	8	0.81	2.08%	50.6	820.96	823.66



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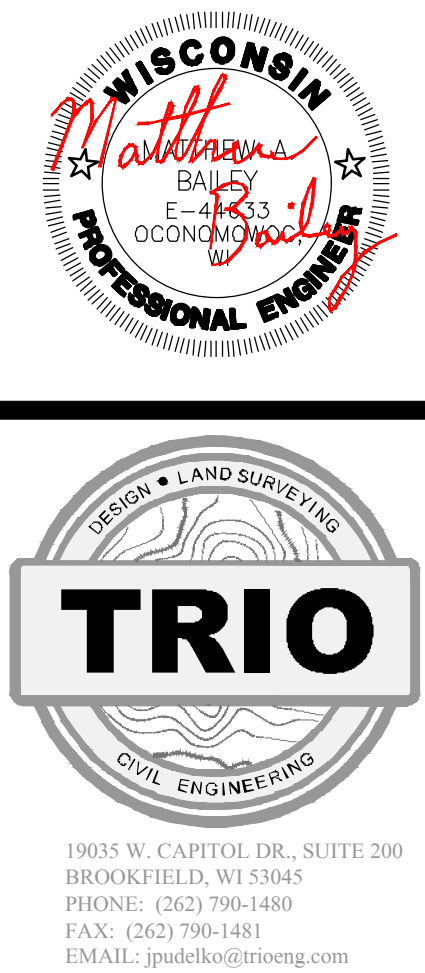
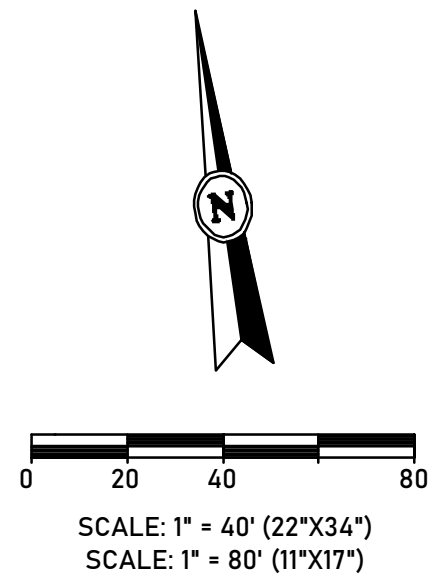
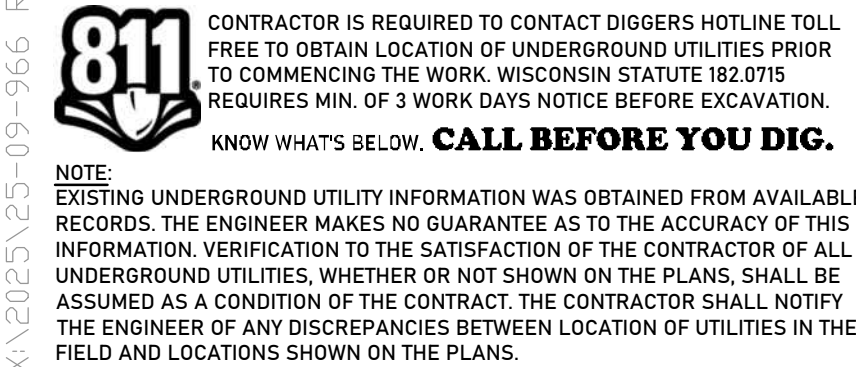
DATE:
NOVEMBER 14, 2025

JOB NUMBER:
25-09-966

DESCRIPTION:
SANITARY
PLAN &
PROFILE

SHEET

C4.1



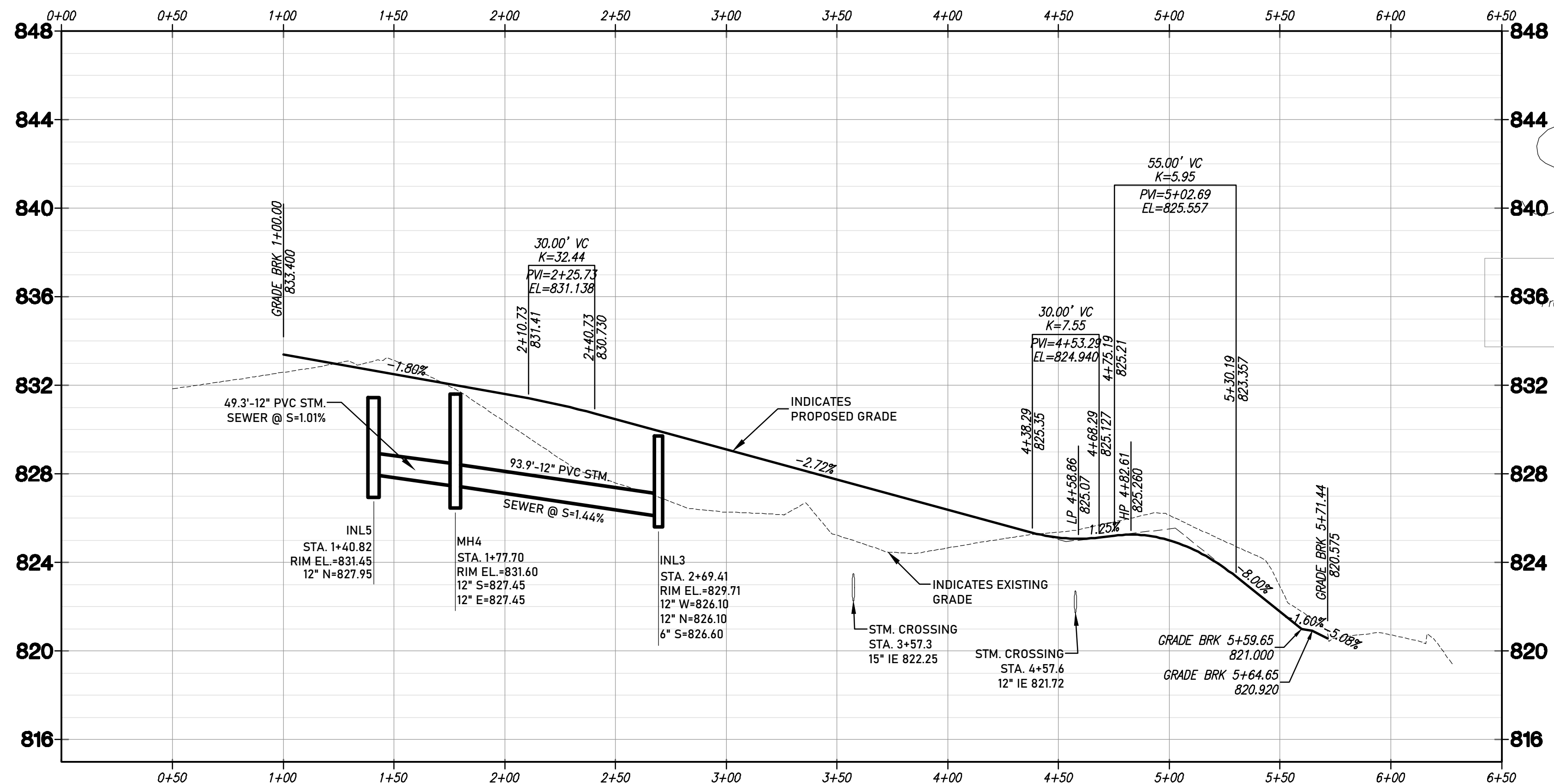
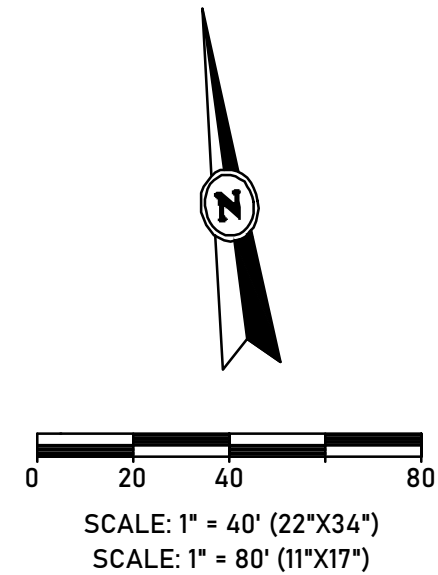
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JOB NUMBER: 25-09-966

DESCRIPTION: WATERMAIN PLAN & PROFILE

SHEET C4.2



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PROJECT:
RIVER ROAD VILLAS
CITY OF WAUKESHA, WISCONSIN
BY: BIELINSKI HOMES
1830 MEADOW LN., SUITE A
PEWAUKEE, WI 53072

REVISION HISTORY

DATE:
NOVEMBER 14, 2025

JOB NUMBER:
25-09-966

DESCRIPTION:
ROAD &
STORM PLAN
& PROFILE

SHEET

C5.0



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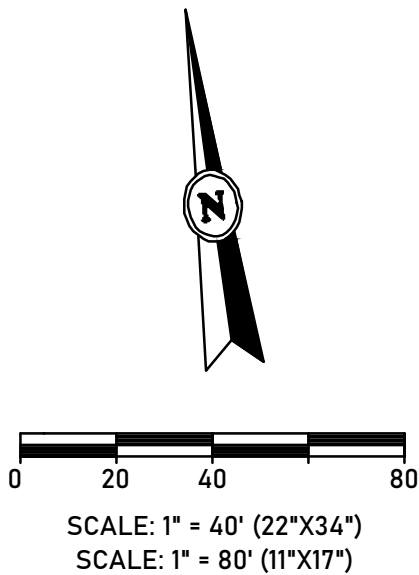
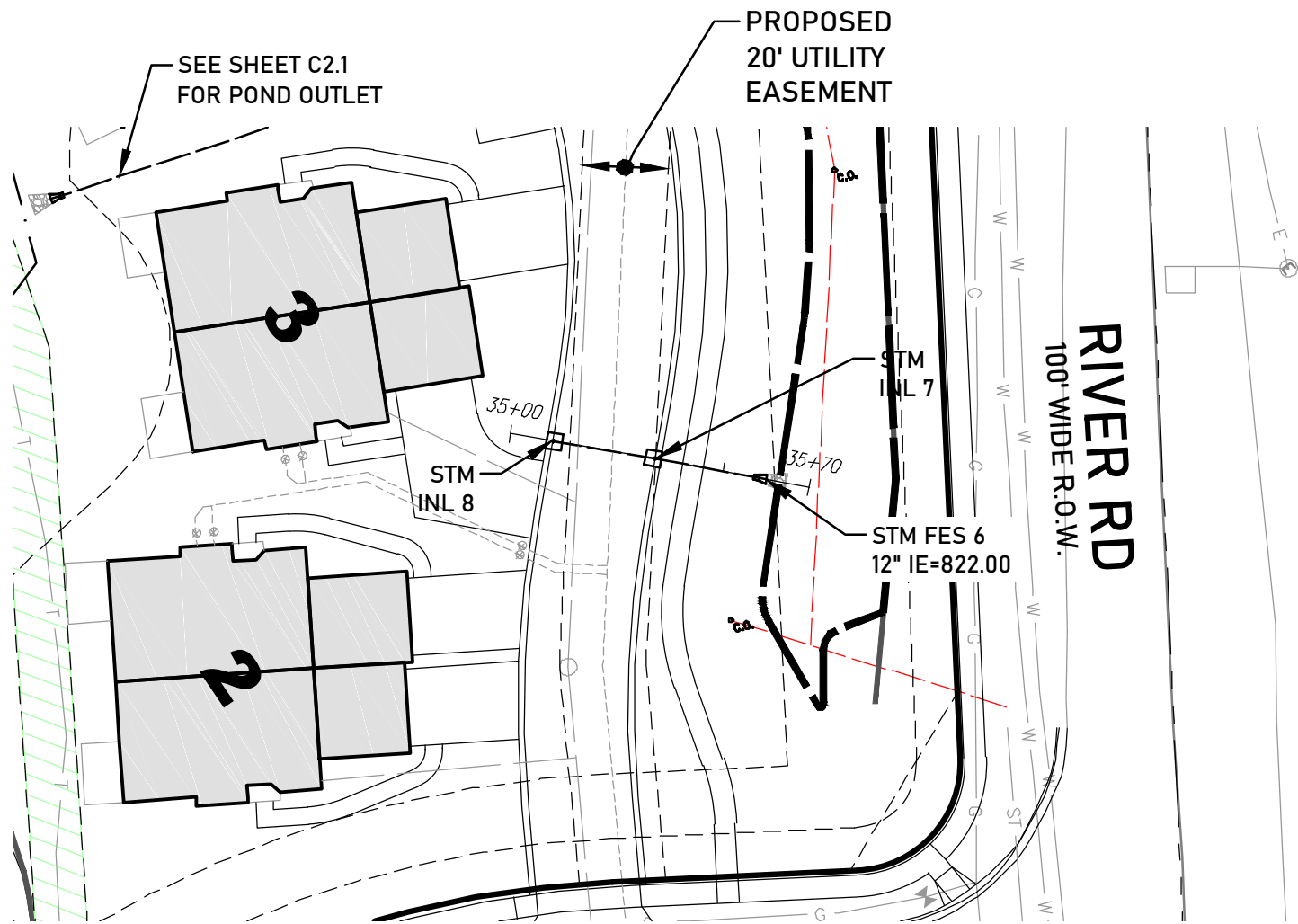
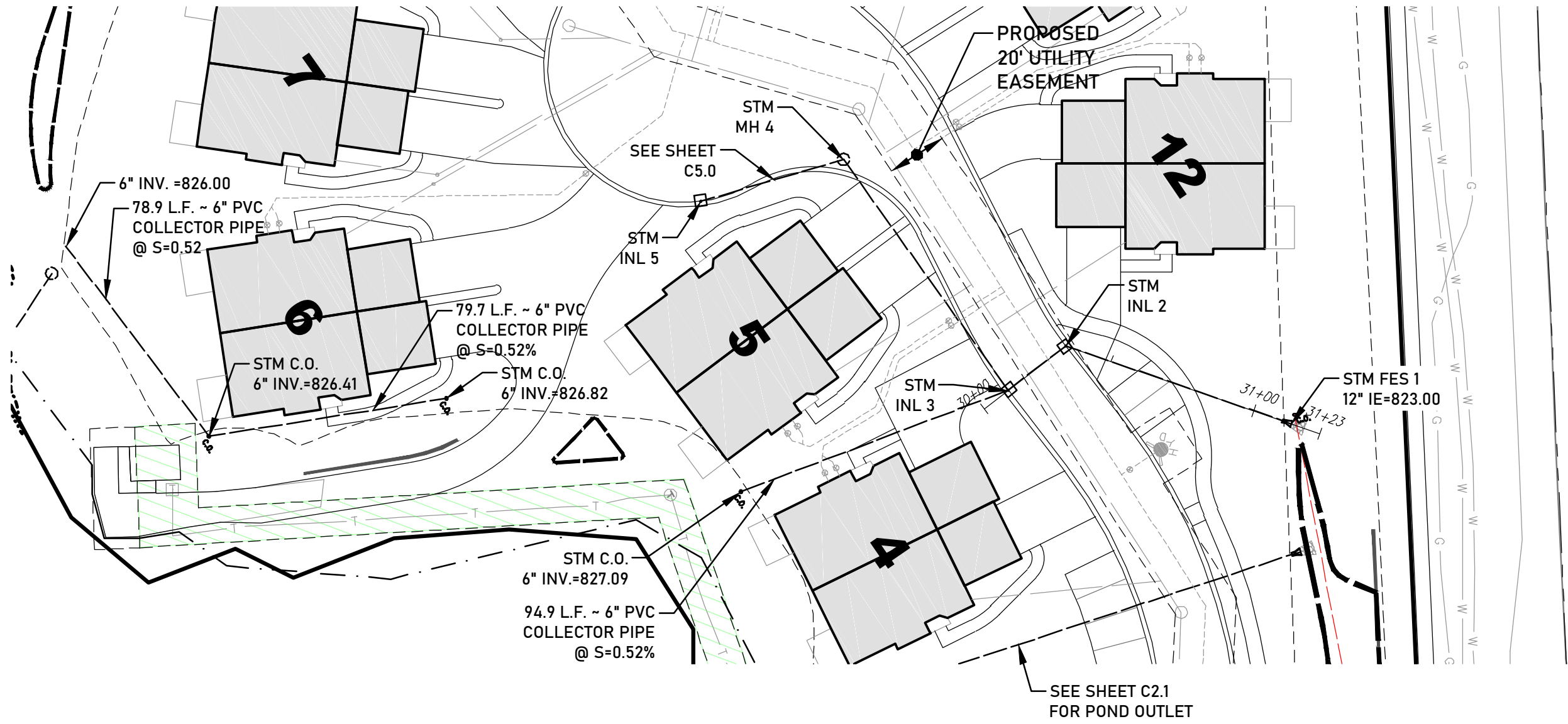
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X:\2025\25-09-966 RIVER ROAD VILLAS - WAUKESHA\DRAWINGS\CONSTRUCTION PLANS\RIVER ROAD VILLAS CIVIL PLAN SET_22X34.DWG



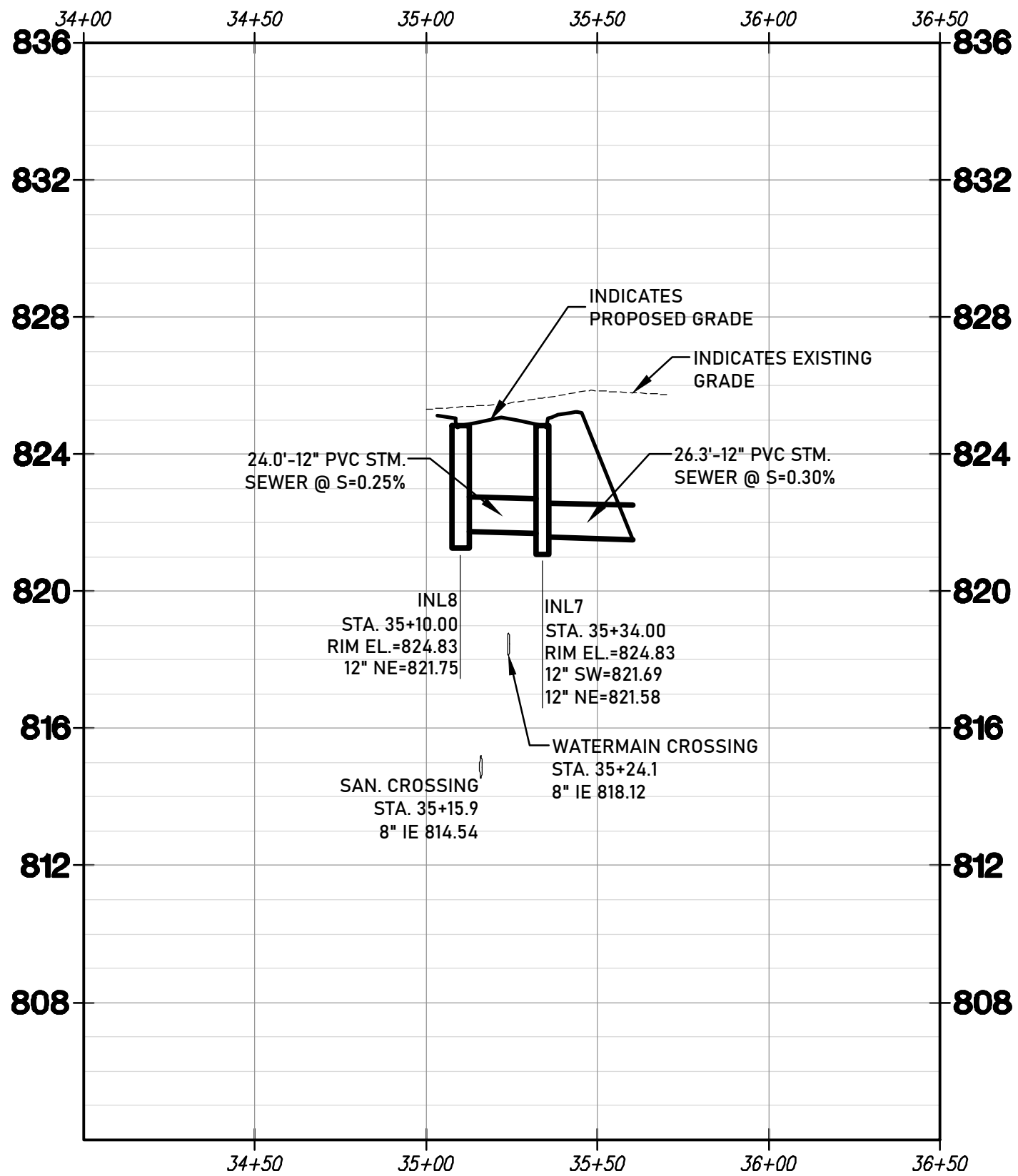
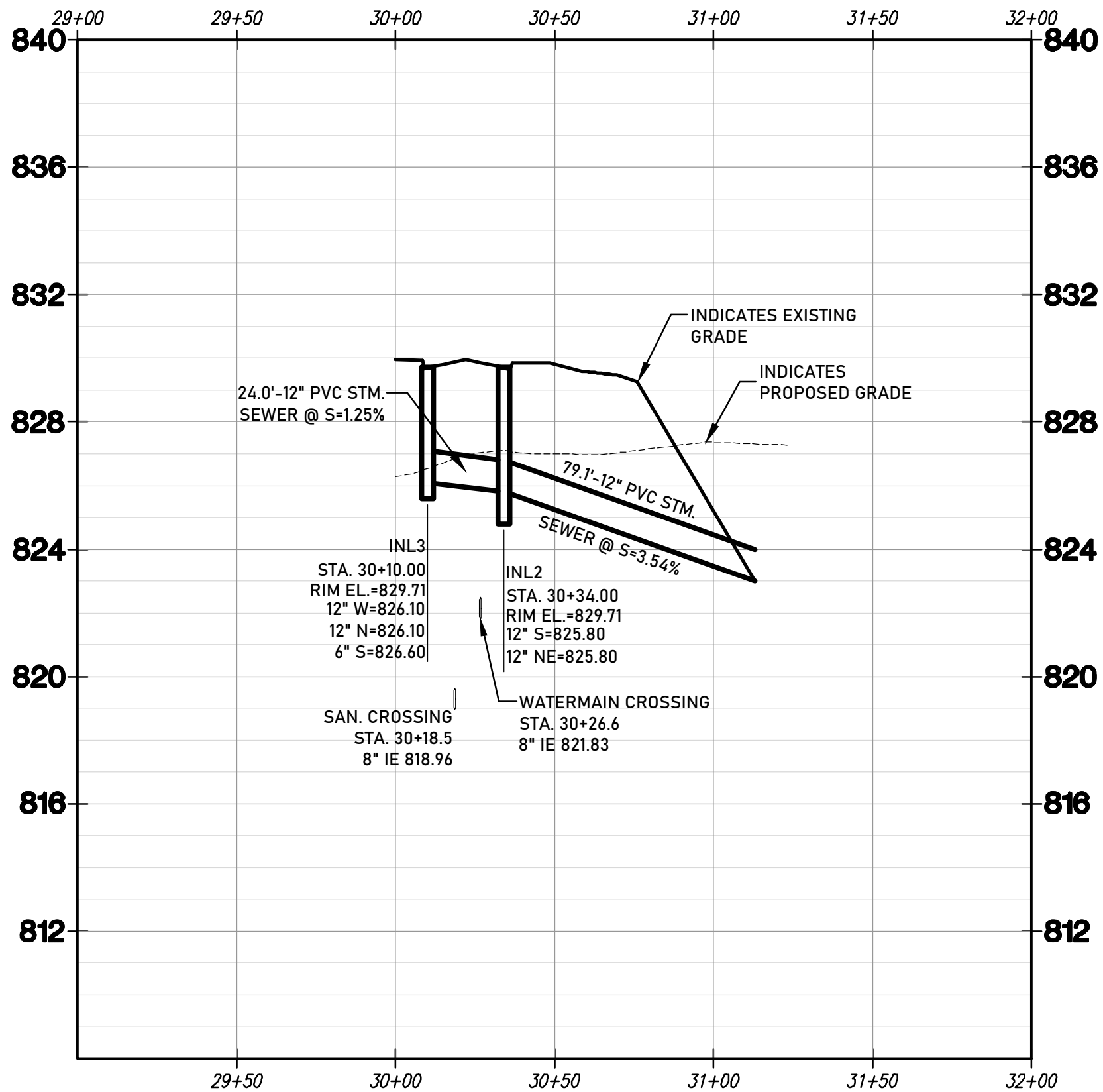
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ROAD & STORM SEWER

FROM: STA: 29+00 TO: STA: 36+50



PROJECT:
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CITY OF WAUKESHA, WISCONSIN
BY: BIELINSKI HOMES
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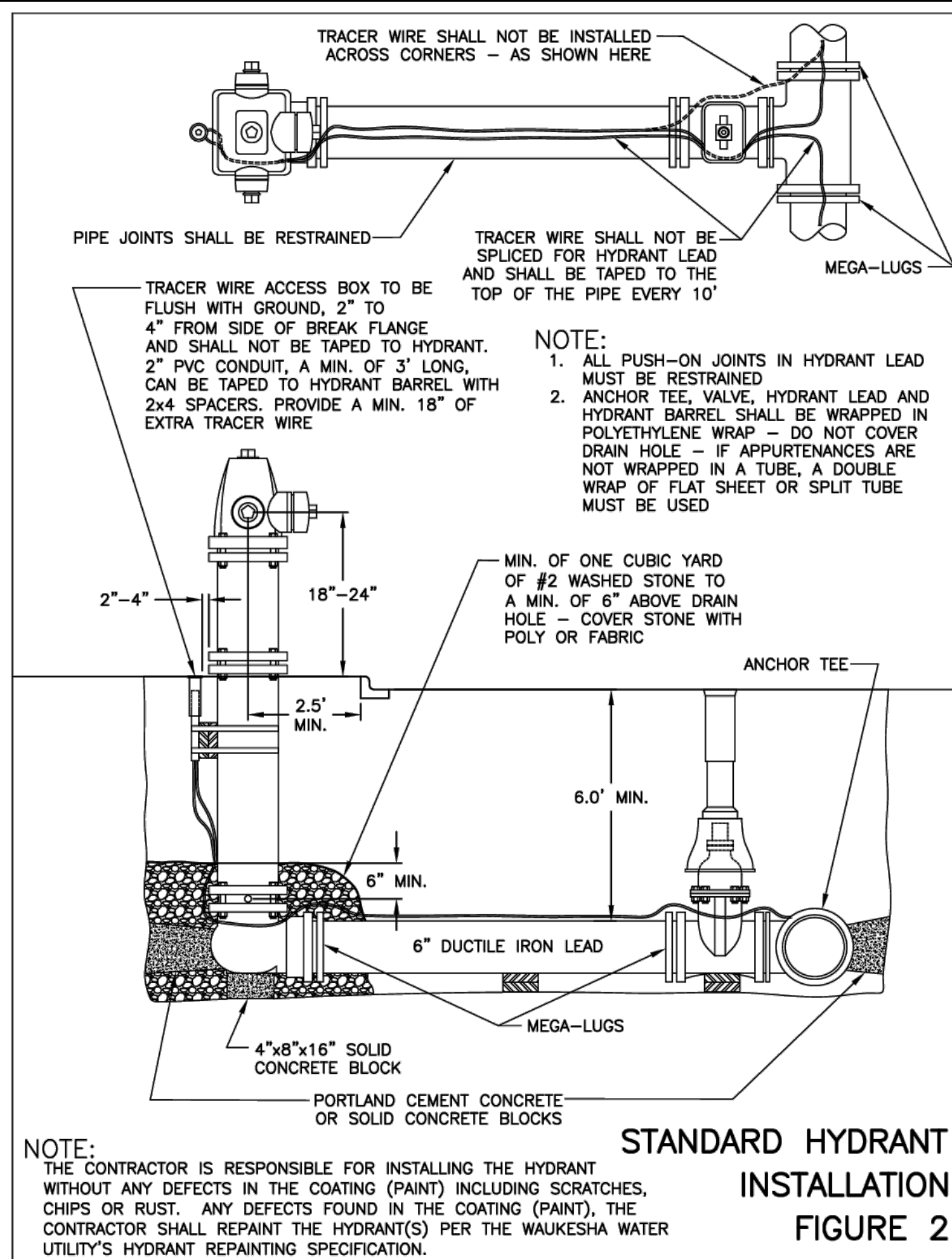
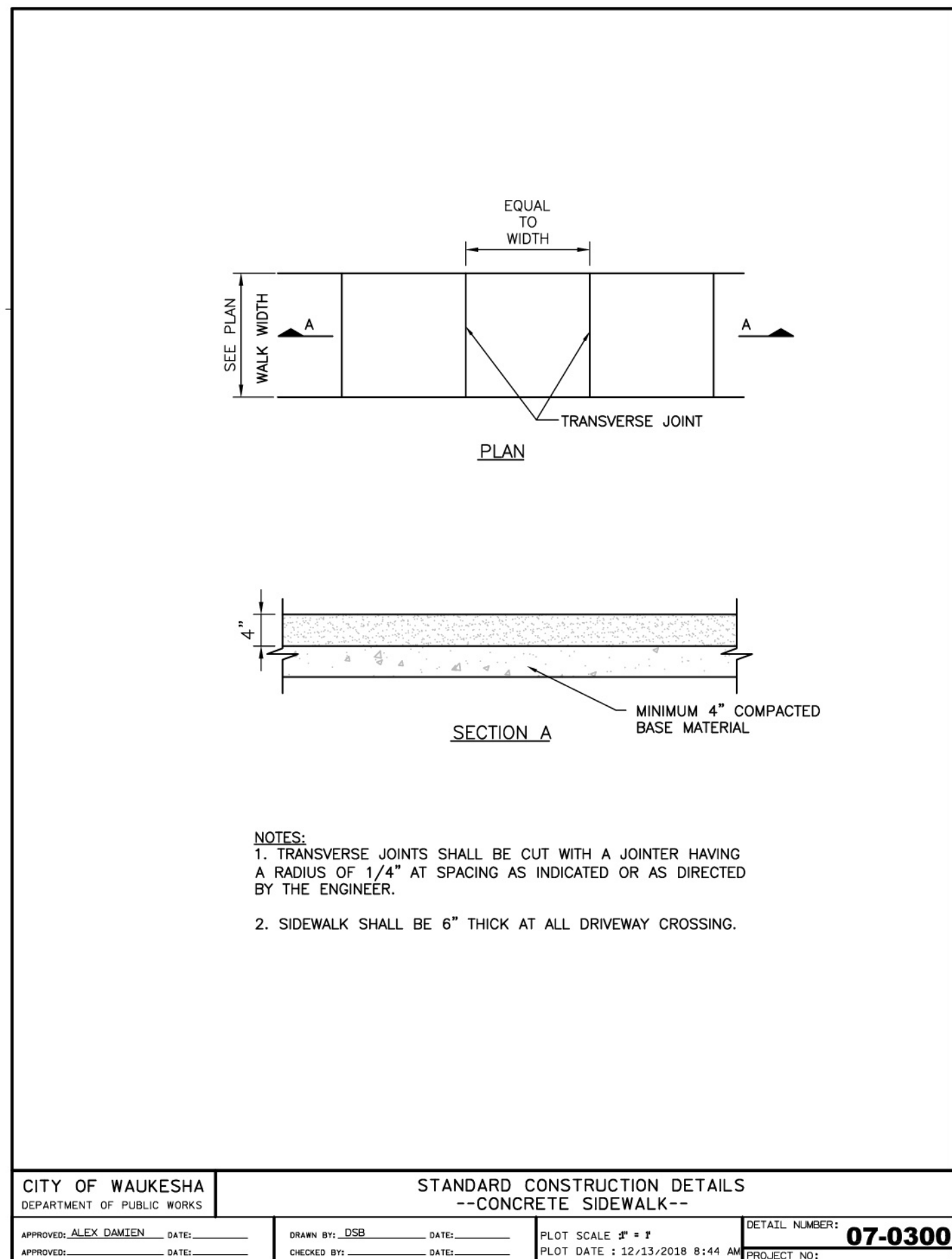
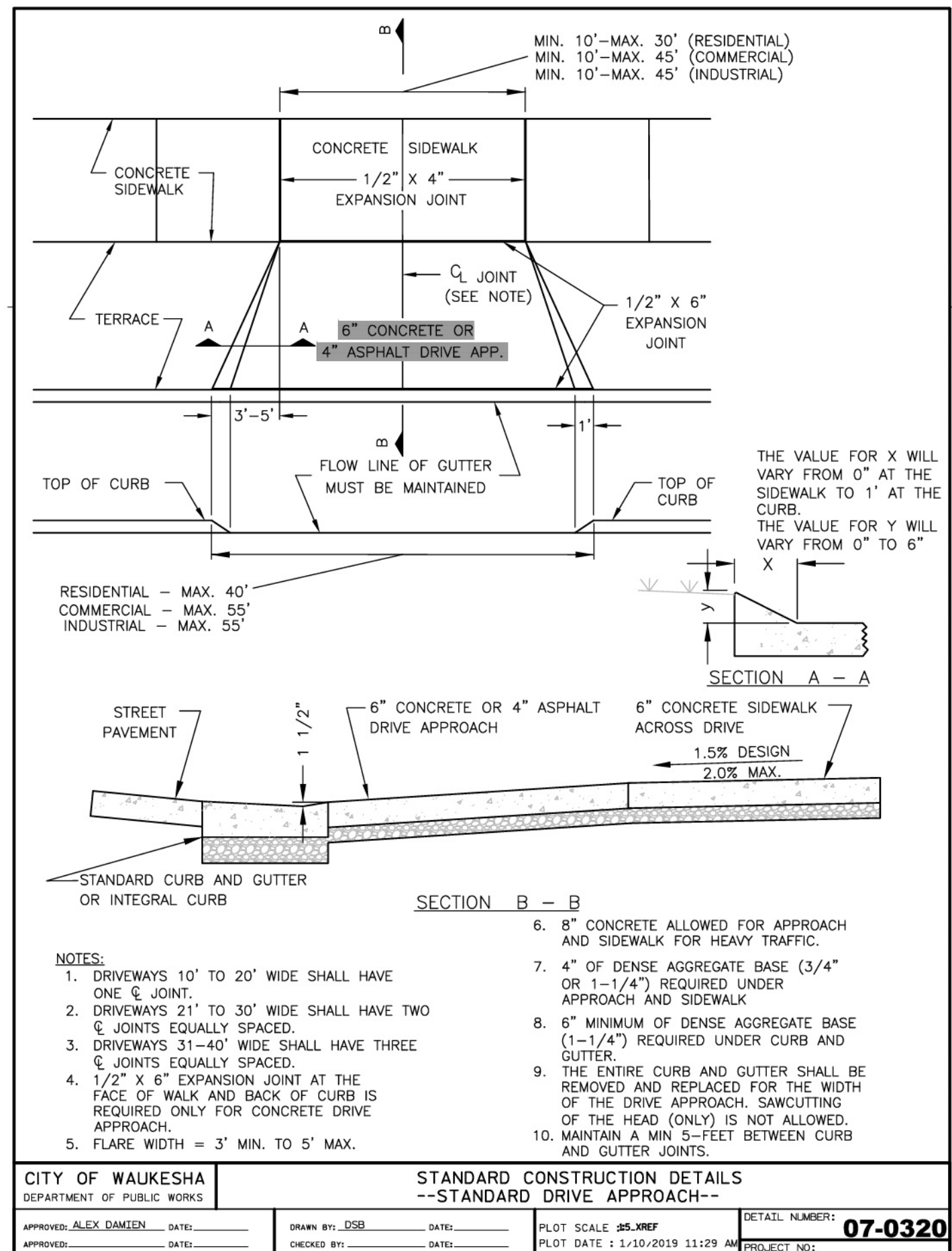
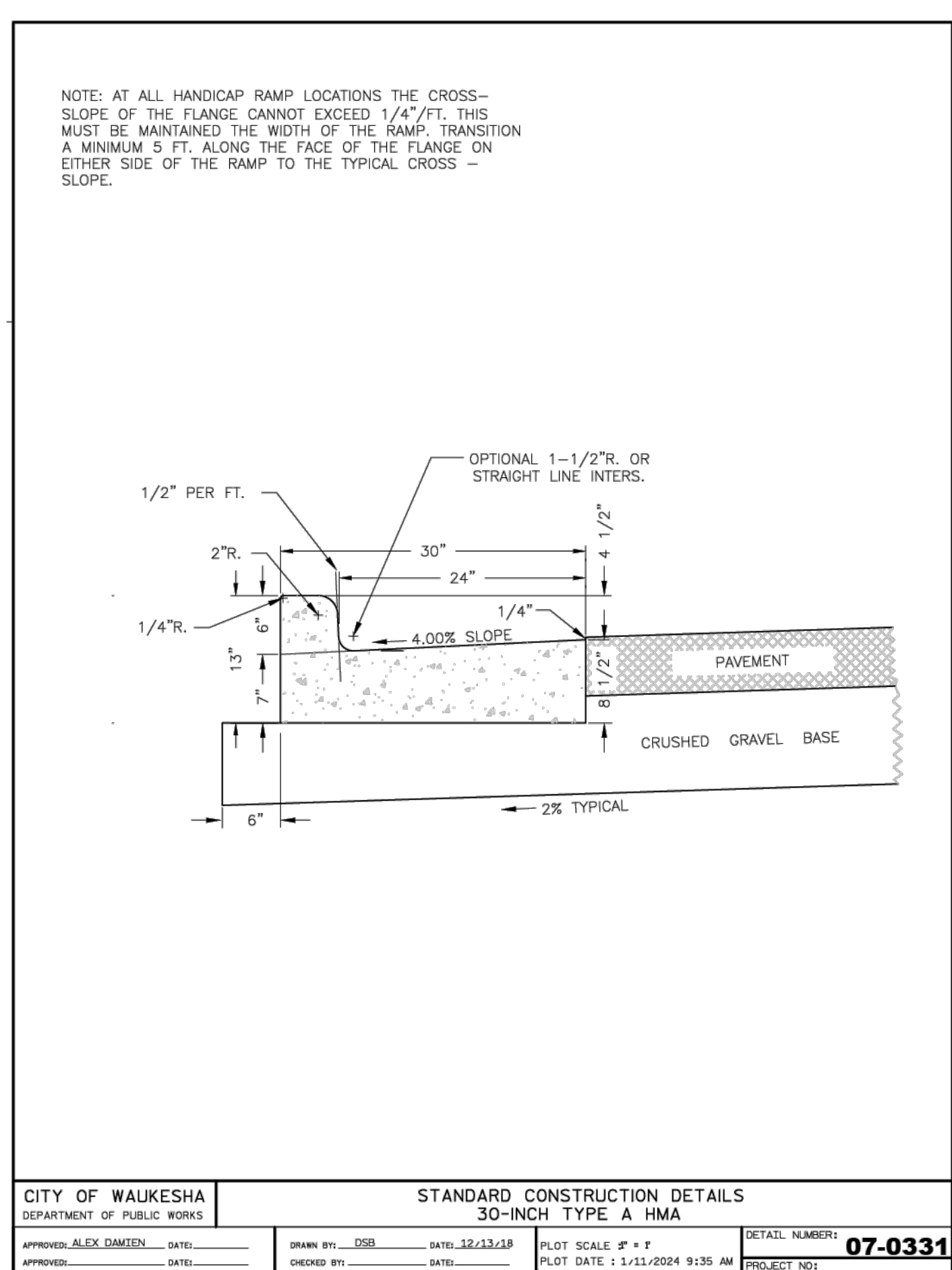
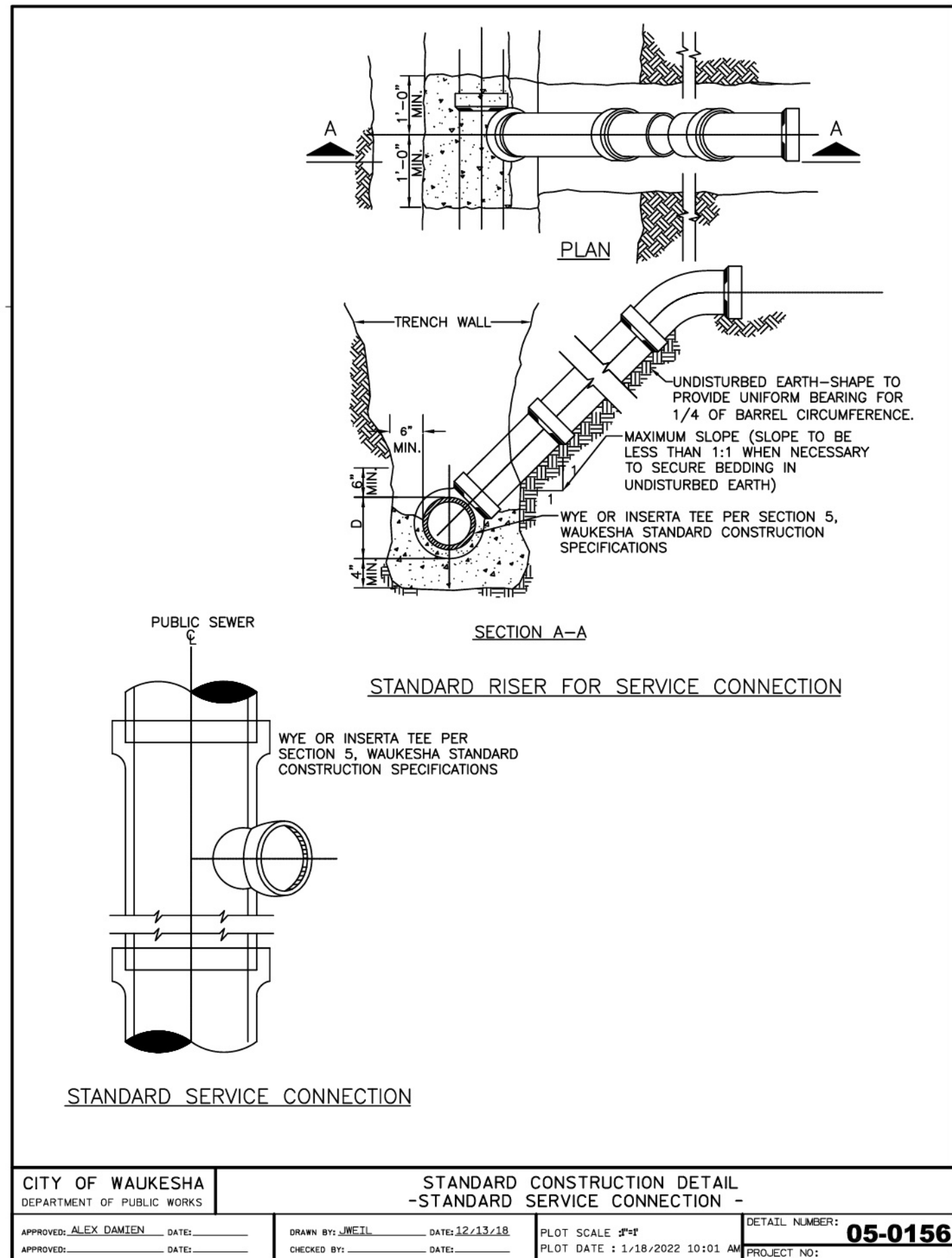
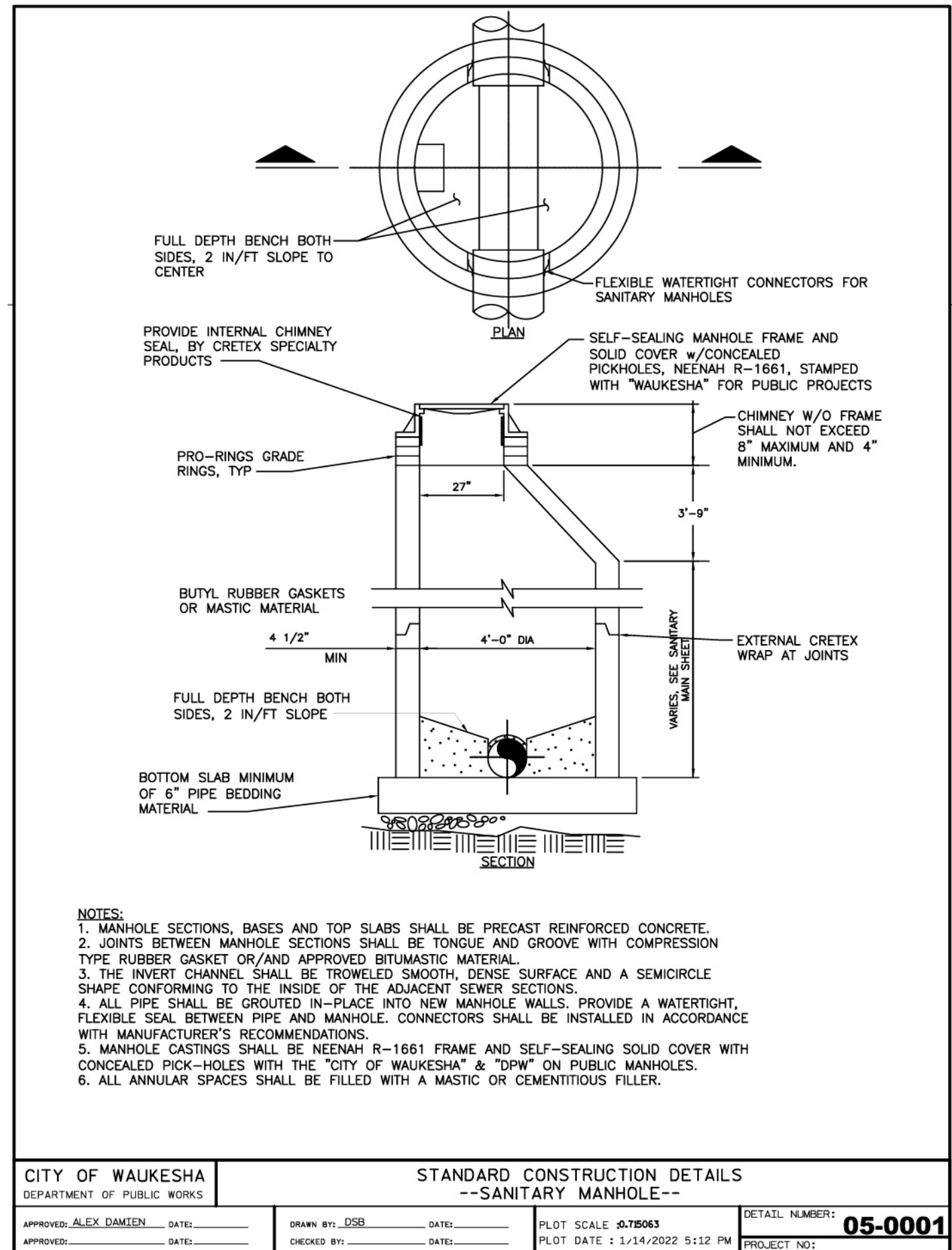
DESCRIPTION:
STORM
SEWER PLAN
& PROFILE


SHEET

C5.1

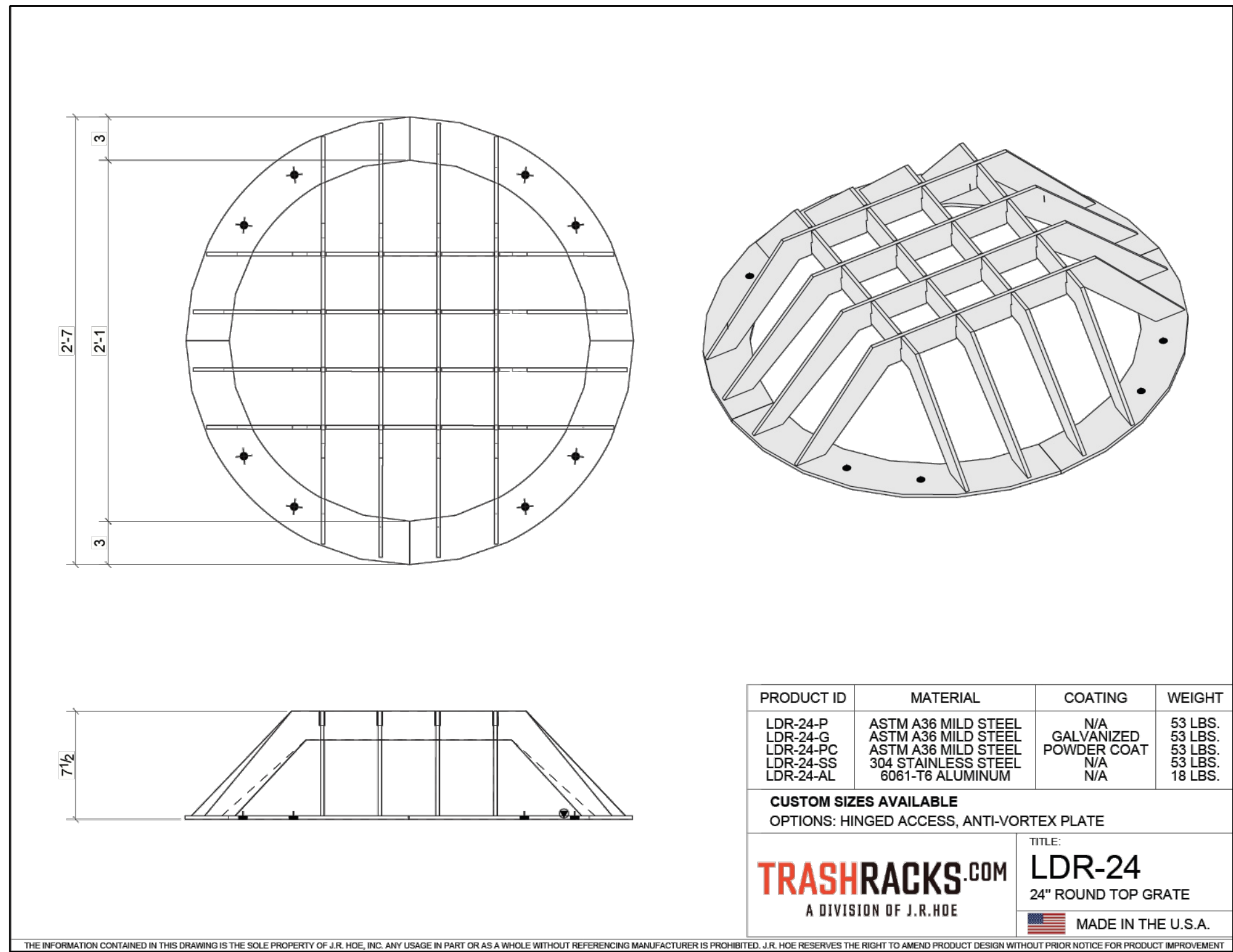
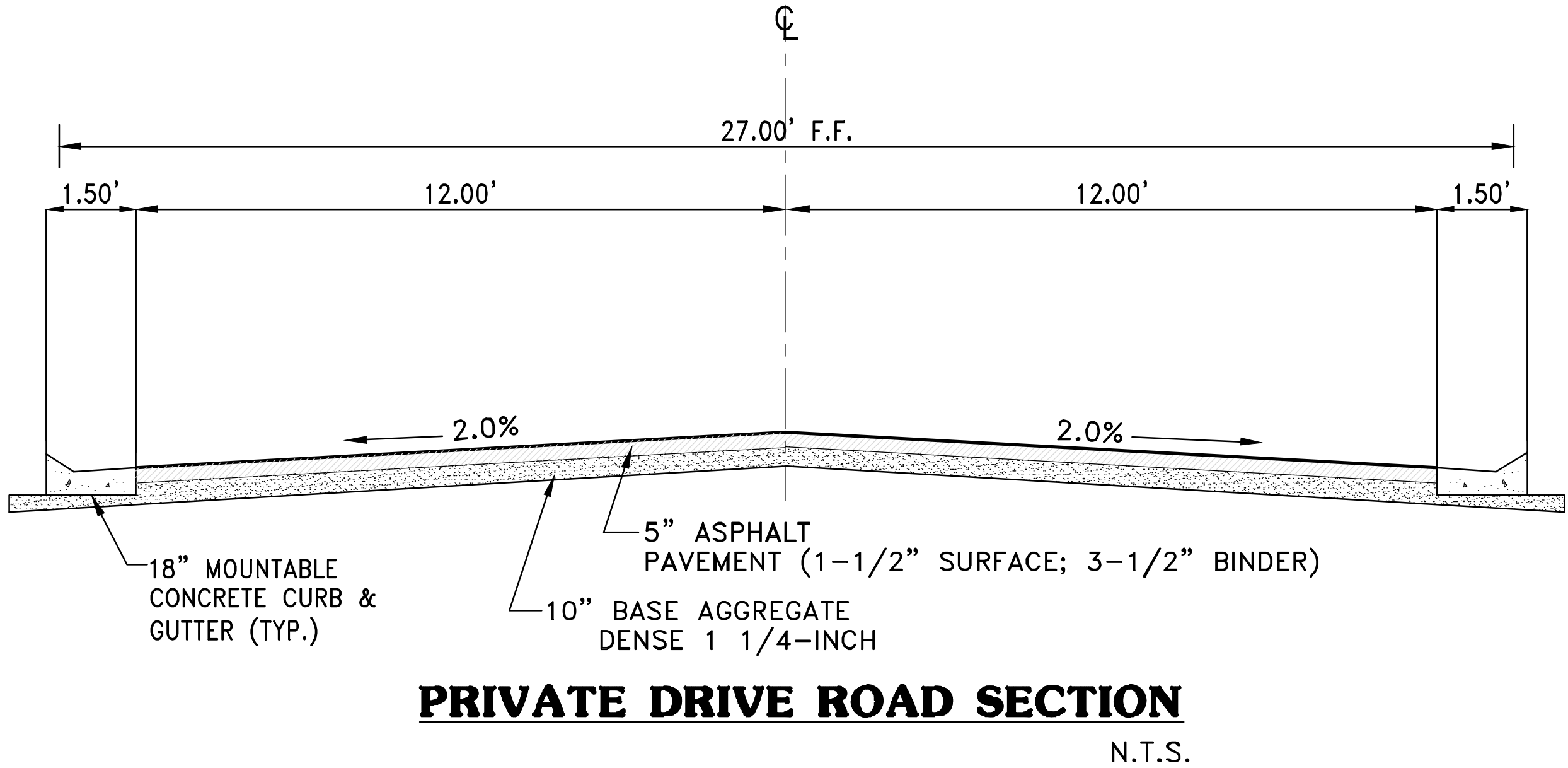
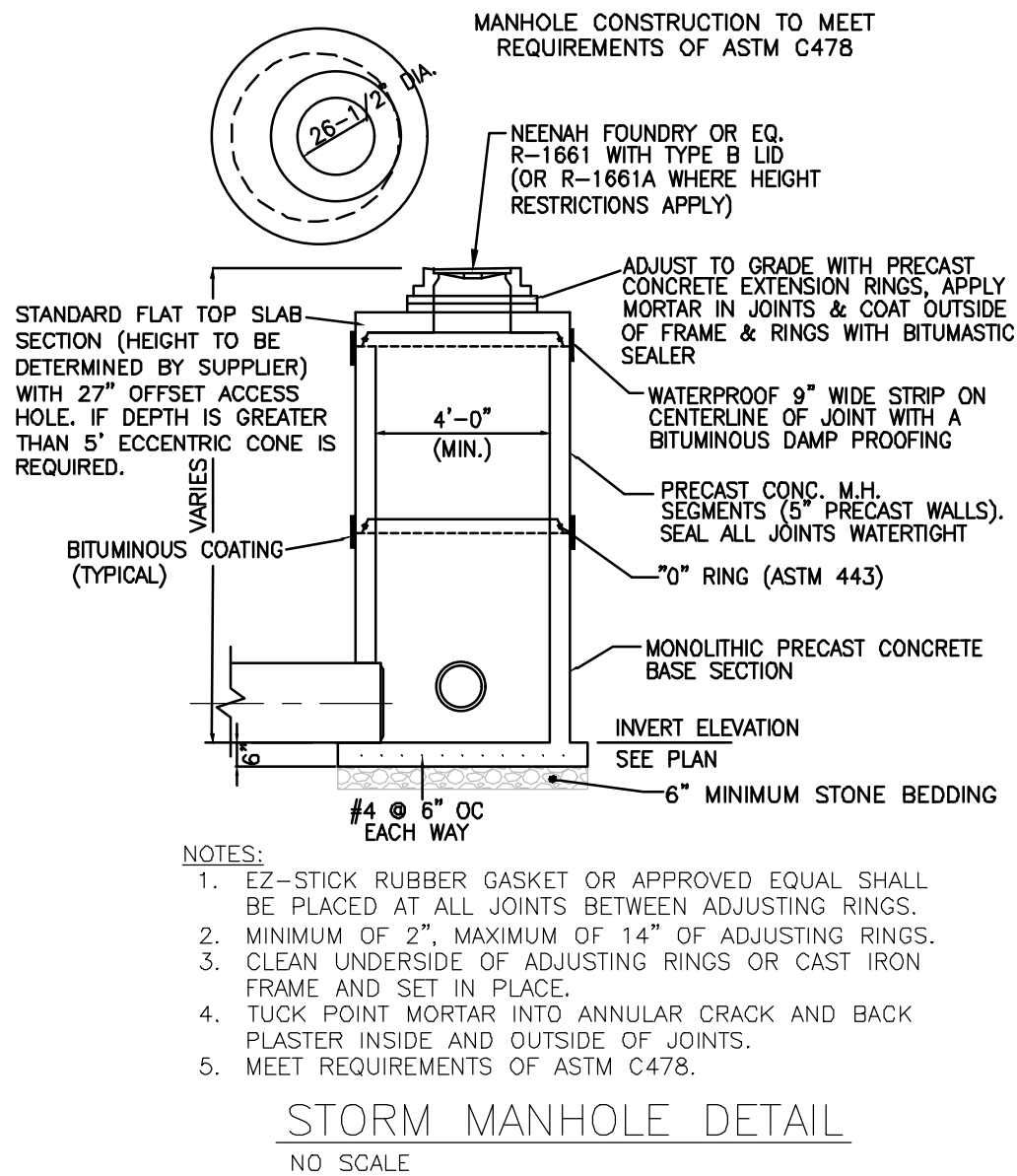
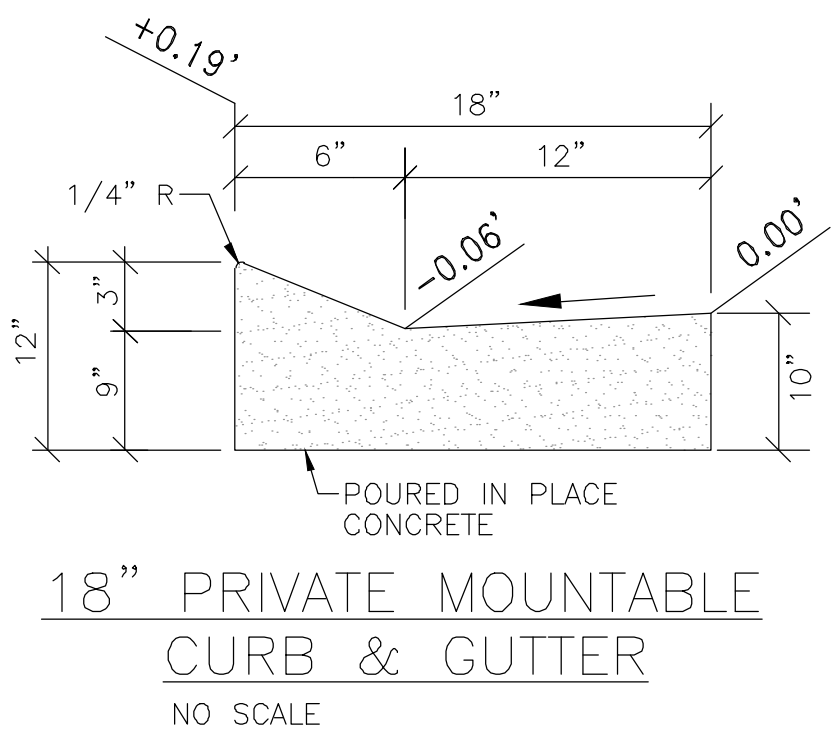
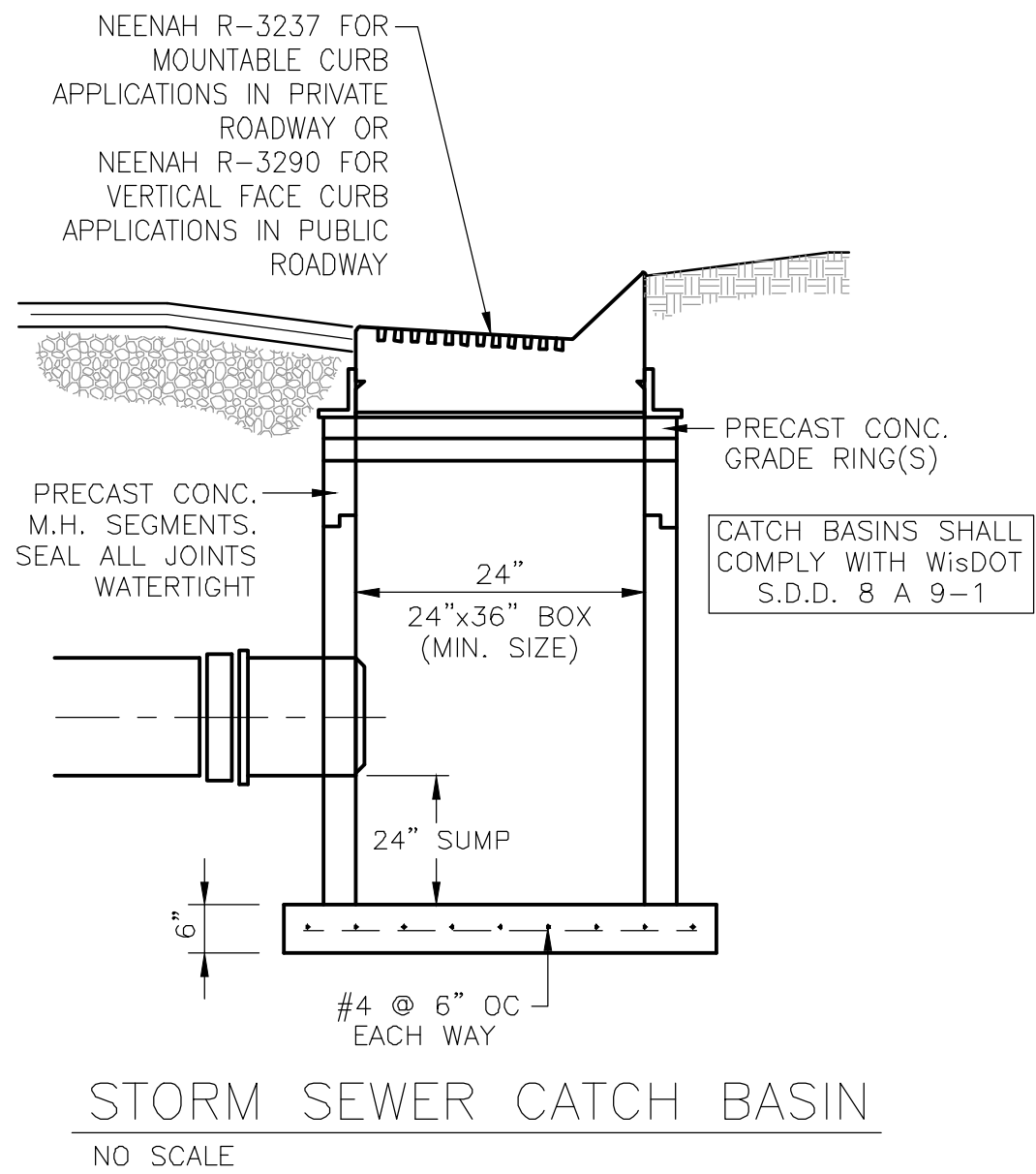
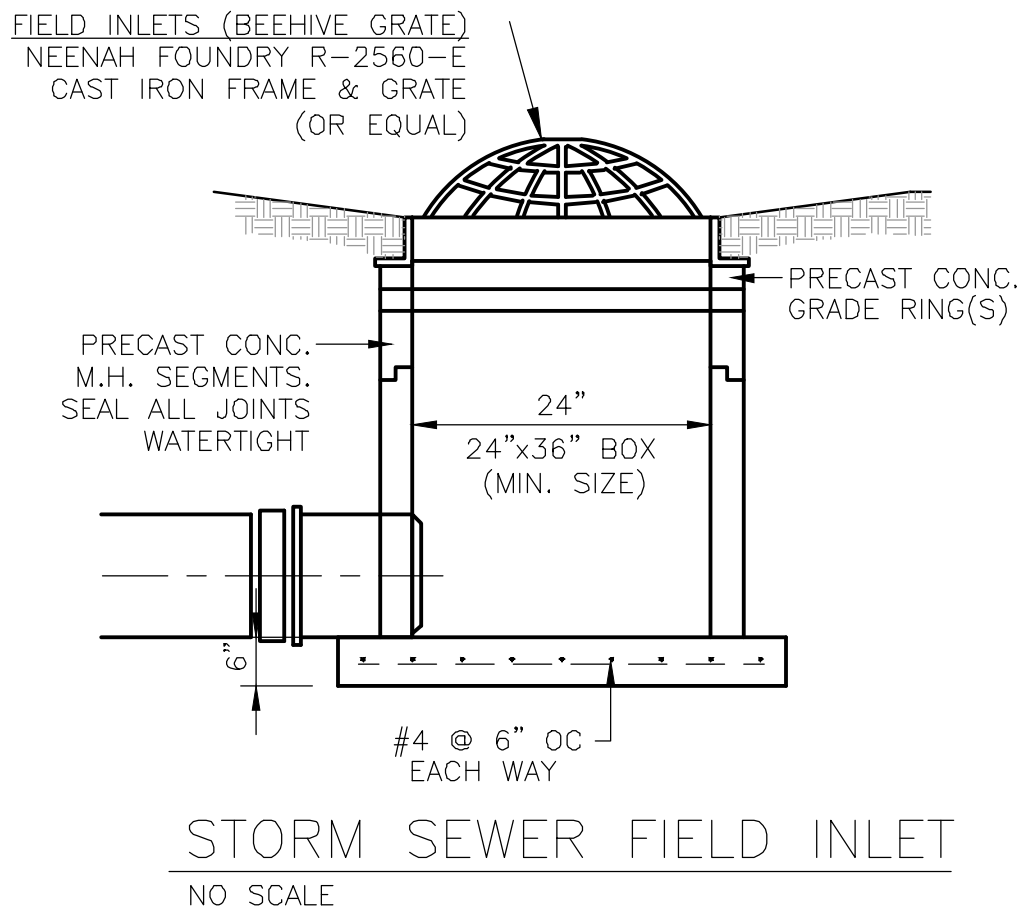


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<div style="display: flex; justify-content: space-between;"> <div style="width: 45%;"> <p>PROJECT:</p> <p>RIVER ROAD VILLAS</p> <p>CITY OF WAUKESHA, WISCONSIN</p> </div> <div style="width: 45%;"> <p>BY: BIELINSKI HOMES</p> <p>1830 MEADOW LN., SUITE A</p> <p>PEWAUKEE, WI 53072</p> </div> </div>	
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JOB NUMBER: 25-09-966	
DESCRIPTION: CONSTRUCTION DETAILS	
SHEET	
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CONSTRUCTION
DETAILS

SHEET

C6.1