

22.06. Parking and Access Standards.

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22.06(1) General Provisions.

A. Purpose. The purpose of this Section is to regulate vehicular, pedestrian, and bicyclist access and mobility in a manner that:

1. Ensures the safety of all transportation system users through design that prioritizes pedestrian and bicyclist movements over automobile movements where appropriate, maintains a safe separation of movements in other cases, and avoids unnecessary or unsafe conflicts,
2. Supports the efficient movement of people and goods through a multi-modal, interconnected transportation network to improve public health and quality of life, support the local economy, and facilitate rapid emergency response,
3. Minimizes negative impacts on the environment through a reduction in carbon emissions associated with automobile use,
4. Prioritizes a human-scaled built environment that accommodates pedestrians to improve physical, mental, and emotional health,
5. Relieves traffic congestion on streets by limiting curb cuts and promoting alternative modes of transportation, and
6. Provides adequate, but not excessive, off-street parking.

B. Applicability. The standards of this Section shall apply as detailed in Table 22.06(1)(B).

Table 22.06(1)(B) Access and Mobility Standards Applicability Overview						
Key:		New development	Major redevelopment	Intermediate redevelopment	Minor redevelopment	Change of use
<ul style="list-style-type: none"> ● = Standards apply to entire development site ○ = Standards apply to portion of site or building(s) impacted by development activity Blank = Standards do not apply 						
Standard	Reference					
Internal Access Drives	22.06(2)	●	●			
Driveways	22.06(3)	●	●	○	○	
Vehicle Parking – Allowed Parking Locations	22.06(4)(A)	●	●	[1]	[1]	
Vehicle Parking – Minimum and Maximum Parking Space Standards	22.06(4)(B)	●	●	●	●	●
Vehicle Parking – Parking Space and Drive Aisle Dimensional Requirements	22.06(4)(F)	●	●	[1]	[1]	
Vehicle Parking – Parking Lot Design	22.06(4)(G)	●	●	[1]	[1]	
Bicycle Parking	22.06(5)	●	●	[1]	[1]	
Pedestrian Walkways and Trails	22.06(6)	●	●			
Notes						
[1] Standards shall apply when new parking lots and/or areas are constructed or existing parking lots are expanded only.						

C. Exceptions. The Plan Commission may authorize a modification, waiver, or reduction of the requirements of this Section should it find that the nature of a proposed use, development, or redevelopment, or the shape or size of a property makes strict compliance impractical, unjust, unfair, or unreasonable. Such exceptions shall be to the least extent necessary to address their cause. Approval of exceptions shall be consistent with all applicable Review Criteria in Subsection 22.11(4) (Plan Commission SPAR).

22.06(2) Internal Access Drive Standards.

- A. Applicability.** Internal access drives shall be required in all developments with uses in the multi- unit residential, public and institutional, and/or commercial use categories and with either five acres or more of gross development site area or 100 or more parking spaces.
- B. Primary Internal Access Drive Standards.** Primary internal access drives shall connect a site to the public right-of-way, provide access to secondary internal access drives, and meet the standards established in Table 22.06(2)(B). If the developer chooses to include elements not required in Table 22.06(2)(B) the elements shall meet the standards of Subsection 22.06(2)(C).

Table 22.06(2)(B) Primary Internal Access Drive Standards	
Required Element	Standards
Travel Lane	Shall be a minimum of 12 feet for one-way access and 24 feet for two-way access.

Table 22.06(2)(B) Primary Internal Access Drive Standards	
Required Element	Standards
Planted Median	Shall be improved with a mountable curb with a minimum height of nine inches and a gutter with a minimum width of nine inches
	Shall taper to a minimum of two feet in width at intersections
	May have breaks to accommodate turning movements
	Shall be planted with a minimum of one canopy tree, two ornamental trees, and 10 evergreen or large deciduous shrubs, small flowering shrubs, or native grasses/forbs for every fifty (50) linear feet.
Curb and Gutter	Per City's Development Handbook
Parkway	Shall meet the parkway standards for minor collector roadways per Development Handbook
Pedestrian Walkway – both sides of drive	Shall meet the sidewalk standards of Subsection 22.06(6)

C. Secondary Internal Access Drive Standards. Secondary internal access drives shall connect primary internal access drives to buildings and parking lots on a site and meet the standards established in Table 22.06(2)(C). If the developer chooses to include elements not required in Table 22.06(2)(C) the elements shall meet the standards of primary internal access drives in Subsection 22.06(2)(B).

Table 22.06(2)(C) Secondary Internal Access Drive Standards	
Required Element	Standards
Travel Lane	Shall meet the travel lane width and number requirements of local roadways (Table #####)
Curb and Gutter	Per City's Development Handbook
Parkway	Shall meet the parkway standards for local roadways per Section #####
Pedestrian Walkway – one side of drive	Shall meet the sidewalk standards of Subsection 22.06(6)

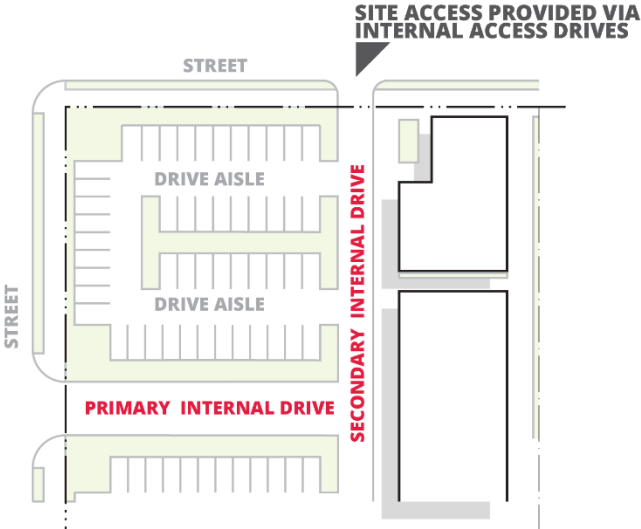


Figure 1 Internal Access Drive Standards

22.06(3) Driveways.

A. Number of Allowed Driveways.

- 1. Residential Neighborhood Districts (RN-1, RN-2, RN-3, and RN-4).** For residential uses, one driveway shall be permitted per lot. Lots with a width greater than 60 feet may be permitted one additional driveway, subject to approval by the Director of Public Works in accordance with departmental guidelines and requirements.
- 2. All Other Use Categories.** Driveways in all other use categories may be limited based on applicable engineering standards.

B. Location of Driveway Access Points.

- 1.** Access shall be taken from an alley if an alley abuts the property requiring access.
- 2.** Access shall be taken from the lower-classified fronting roadway where a through lot or corner lot fronts roadways of different functional classifications, unless one or more of the following conditions apply:
 - a.** The frontage along the lower-classified roadway is less than 50 feet in width,
 - b.** The shape of the parcel and/or placement of existing buildings would require the driveway to be 50 or more feet longer than if the access was taken from the higher- classified street,
 - c.** The lower-classified roadway primarily serves uses in the neighborhood residential use category and the use of the subject lot is in any other use category, or
 - d.** If necessary to protect public safety.

C. Driveway Setbacks.

- 1.** For lots less than 60 feet in width, a minimum setback of two feet shall be required from any lot line other than a right-of-way line.
- 2.** Shared driveways may be permitted with an access easement recorded with the Waukesha County Register of Deeds. No minimum lot line setback will apply to shared driveways.
- 3.** For lots greater than 60 feet in width, a minimum setback of five feet from any property line other than a right-of-way shall be required.

- a. The Director of Public Works or their designee shall have the authority to restrict driveway access to locations deemed appropriate for the operational needs of the site and existing infrastructure, such as protecting the function, safety, and efficiency of travel on the roadway and any associated bicycle and pedestrian facilities. There is no guarantee of access to the applicant’s preferred driveway location or access point.
- b. The minimum distance between driveways/curb cuts, as measured from back of curb to back of curb (or back of pavement to back of pavement if no curb is present), on the same side of the street shall be in accordance with Table 22.06(3)(D) The required separation shall be based on the roadway classification of the higher-classified roadway within the intersection.

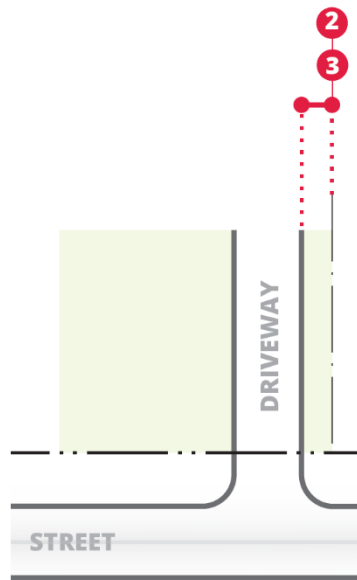


Figure 2 Driveway Setback

D. Driveway Dimensions.

- 1. **Driveway Width.** The minimum and maximum width of a driveway, as measured at the property line and at the public right-of-way line, shall comply with the standards set forth in Table 22.06(3)(D).

Table 22.06(3)(D) Minimum and Maximum Driveway Width [1]		
Uses	Minimum Width	Maximum Width
One- and Two-Unit Residential	8 ft	20 ft [2]
Multi-Unit / Mixed-Use / Commercial / Institutional	12	36
Industrial / Agricultural / Office	16	50
Notes		

[1] Driveways serving one- and two-unit residential uses shall include a minimum parking space measuring 8 feet by 18 feet. Driveways serving all other use categories shall not be subject to a minimum length requirement.
 [2] Or 40 percent of lot width, whichever is lesser, if the driveway is located in the front yard of a property in the residential neighborhood districts.



Figure 3 Driveway Width

22.06(4) Vehicle Parking.

A. Allowed Parking Locations. The allowed location of accessory parking lots shall be in accordance with Table 22.06(4)(A).

Table 22.06(4)(A) Allowed Parking Locations			
District	Front and/or Street Side Yard [1]	Interior Side Yard	Rear Yard
RN-1, RN-2, RN-3, RN-4		•	•
MU, DC			•
GC, CC, I	•[2]	•	•
LM, GM, OM, MM	•	•	•
Notes			
[1] Parking of residential vehicles on conforming residential driveways shall be exempt from front/street side yard parking prohibitions. All residential vehicles parked on a conforming driveway shall not block any pedestrian or utility infrastructure.			
[2] If a parking lot is located in a front and/or street side yard, the parking lot shall be separated from the principal building(s) by a secondary internal access drive meeting the standards of Subsection 22.06(2)(C).			

B. Minimum and Maximum Parking Space Standards.

1. Multi-Use Development Sites. The total number of required parking spaces for development sites containing more than one use shall be determined by adding together the required number of spaces for each proposed use as if each use were calculated separately.

2. **Use Undetermined.** If the ultimate use(s) of a proposed building(s) is not determined at the time of development approval, the Community Development Director or their designee shall determine which use to utilize in the calculation of minimum required and maximum allowed parking based on the district in which the development is located and the purpose of this Subsection.

3. **Minimum Parking Requirement.** The number of required off-street parking spaces shall be calculated according to the formulas established in Table 22.06(4)(B)(3). Nonresidential uses in MU and DC districts are exempt from minimum parking requirements.
 - a. **Per Square Footage.** “Sq ft” shall mean the sum of the gross leasable horizontal area of all floors of a nonresidential building or nonresidential portion of a mixed-use building, not including storage or mechanical space.

 - b. **Per Other.** Other variables are measured according to their common meanings.

 - c. **Rounding.** When the calculation of required parking spaces results in a fraction, the total number of required parking spaces shall be rounded up to the nearest whole number.

Table 22.06(4)(B)(3) Minimum Parking Required per Use	
Use	Minimum Parking Spaces Required
Residential Principal Uses	
Community Living Arrangement, 8 or fewer residents	0.5 per unit per resident capacity
Community Living Arrangement, 9 or more residents	0.5 per unit
Dwelling, Cottage Home/ Tiny Home Court	1 per bedroom or 2 per unit, whichever is less
Dwelling, Duplex; Side-by-side	2 per unit
Dwelling, Duplex; Stacked	
Dwelling, Live-Work Unit	2 per unit
Dwelling, Mobile Home Park	1.5 per unit
Dwelling, Multi-Unit Building Complex	1.5 per Unit
Dwelling, Multi-Unit, above ground floor only	
Dwelling, Multi-Unit, more than 8 units	
Dwelling, Multi-Unit, Rear Ground-Floor	
Dwelling, Multi-Unit, 5-8 units	
Dwelling, Quadplex, Triplex, Rowhome	1.5 per Unit
Dwelling, Single-Family, Detached	2 per unit
Foster Family Home	2 per unit
Public and Institutional Principal Uses	
Alternative Education Facilities	1 per 300 sq ft
Cemetery	n/a
Clinic	1 per 300 sq ft

Table 22.06(4)(B)(3) Minimum Parking Required per Use	
Use	Minimum Parking Spaces Required
College/University	1 per 500 sq ft
Community Services Facility	
Crematory	1 per 300 sq ft
Funeral Home	n/a
Hospital	1 per 500 sq ft
Nursing Home	1 per 4 Beds
Parks and Playgrounds	1 per 1,000 active recreation sq ft
Place of Worship	1 per 500 sq ft
Post Office	1 per 300 sq ft
Private Place of Assembly	1 per 500 sq ft
School, K-12	
Theater	1 per 250 sq ft
Vocational / Technical School	1 per 500 sq ft
Commercial Principal Uses	
Adult-Oriented Establishment	1 per 250 sq ft
Animal Boarding Facility	1 per 500 sq ft
Animal Hospital	
Veterinarian Clinic	1 per 300 sq ft
Artisan Manufacturing	1 per 1,000 sq ft
Bar/Lounge/Tavern	1 per 200 sq ft
Bed and Breakfast/Short Term Rentals	1 per 4 guests
Brewery/Winery/Distillery	1 per 1,000 sq ft
Coffee Shop	1 per 200 sq ft
Commercial Adult and Child-care Centers	1 per 300 sq ft
Commercial Kitchen	
Commercial Recreation, Indoor	1 per 500 sq ft
Commercial Recreation, Outdoor	1 per 500 sq ft
Co-Working Space	
Financial & Professional Services	1 per 300 sq ft
Food Truck Court	2 per food truck
General Office	1 per 500 sq ft
General Retail (Greater than or equal to 15,000 SF)	1 per 500 sq ft
General Retail (less than 15,000 SF)	
General Services	
Grocery Stores (greater than 25,000 SF)	1 per 300 sq ft
Grocery Stores (less than 25,000 SF)	
Hotels and Motels	1 per guest room
Meeting/Event Facility	1 per 300 sq ft
Micro- Brewery/Winery/Distillery	1 per 200 sq ft
Personal Services	1 per 300 sq ft
Restaurant	1 per 200 sq ft

Table 22.06(4)(B)(3) Minimum Parking Required per Use	
Use	Minimum Parking Spaces Required
Industrial Principal Uses	
Auto Dealership and Rental	1 per 1,000 sq ft
Car Wash	2 per car wash lane
Fleet Fueling Facility	1 per 1,000 sq ft
Garden Center (With Outdoor Storage)	1 per 300 sq ft
General Manufacturing	1 per 1,000 sq ft of office space
Heavy Industrial, Indoor	
Heavy Industrial, Outdoor	
Light Industrial, Indoor	1 per 500 sq ft
Light Industrial, Outdoor	
Self-storage Facility	1 per 20 storage units
Utility Infrastructure	n/a
Vehicle Fueling Station	1 per 300 sq ft of retail area + 1 per 200 sq ft of restaurant area
Vehicle Services - Major Repair/Body Work	0.5 per service stall
Vehicle Services – Minor Maintenance/Repair	1 per service stall
Wireless communications facility	n/a
Accessory Uses	
Accessory Building/Structure	n/a
Accessory Commercial Units	1 per unit
Accessory Commercial Playground	1 per 200 sq ft of Recreational Space
Accessory Dwelling Unit	1 per unit
Accessory Service Facilities	n/a
Automated Teller Machine (ATM)	
Child Day Care Home	
Construction-Related Activities	
Contractor's Office	1 per 500 sq ft
Contractor's Yard	n/a
Drive-Through Facility	
Fleet Vehicle Storage/ Maintenance	
Garage/ Rummage Sales	
Home occupations	
Model Home	
Pop up Retail/Restaurant	1 per 200 sq ft

Table 22.06(4)(B)(3) Minimum Parking Required per Use	
Use	Minimum Parking Spaces Required
Outdoor Display/ Sale of Merchandise	n/a
Outdoor Seating	
Outdoor Storage	
Solar Energy Collection System, Canopy	
Solar Energy Collection System, Ground Mounted	
Solar Energy Collection System, Roof Mounted	
Swimming Pool, Private	
Wind Energy Conversion System, Roof Mounted	
Wind Energy Conversion System, Ground Mounted	

4. **Maximum Parking Allowance.** An accessory parking lot shall not provide more than 50 percent above the minimum number of spaces required for the use it serves.

C. Allowable Deviations.

1. **Deviations from Minimum Parking Requirements.** A maximum of one deviation from the minimum parking requirements detailed in Table 22.06(4)(B)(3) may be approved per property in accordance with Table 22.06(4)(C).

Table 22.06(4)(C) Allowable Deviations from Minimum Parking Requirements [1]		
Type	Criteria	Adjustment
Tree Preservation	Reduction is necessary to preserve a high-value tree from being damaged or removed and/or to protect tree save areas.	Parking space equivalent to the square footage of tree or tree canopy at maturity.
Transit	Use is located within 1,000 feet of a public transit stop or station and is connected to the stop or station via a continuous sidewalk and/or pedestrian walkway system.	20 percent maximum reduction in required parking.
Shared Use Parking	The parking lot is under a Shared Parking Agreement.	A maximum of 60 percent of the required parking spaces may be shared.
On-Street Parking	Single-Family Detached dwelling or Duplex dwelling is located along one or more public street frontages where public parking is permitted, and which have bump outs.	A maximum of one required parking space may be credited for one legal on-street parking space immediately abutting the subject property. Where a partial space straddles an extension of a side property line, the space may be counted by the abutting property owner in front of whose property 50 percent or more of the space is located.

Table 22.06(4)(C) Allowable Deviations from Minimum Parking Requirements [1]		
Type	Criteria	Adjustment
	Multi-unit use is incorporated into and visually integrated with the street design of one or more public street frontages where public parking is permitted and which have bump outs and chokers, in accordance with the Standards and Specifications, or a parking permit system is in place.	One required parking space may be substituted if one legal on- street parking space immediately abuts the subject property. Where a partial space straddles an extension of a side property line, the space may be counted by the abutting property owner in front of whose property 50 percent or more of the space is located. A maximum of 10 percent of required parking may be adjusted.
Efficiency Units	The dwellings of a triplex/quadplex or multi-unit building meet the definition of an efficiency unit.	1 parking space shall be required per efficiency unit.
Affordable Units	A multi-unit residential dwelling is deed restricted to be affordable to households making 60 percent or less of the Waukesha County median income.	Maximum reduction of 1 parking space per deed restricted affordable unit.
Parking Study	If a professionally prepared parking study is submitted showing less parking than the minimum is required.	Parking may be adjusted as identified in the parking study.
Notes		
[1] Deviation from minimum parking requirements shall not apply to single-family detached or duplex dwellings.		

D. Shared and Joint Use Parking.

1. Shared Parking.

- a. One parking lot may contain required parking spaces for several different uses so long as the required parking spaces are located within 500 feet of the associated development site.
- b. Enforcement of the use of parking spaces in a shared parking lot shall be the responsibility of the property owner.

2. Joint Use Parking. In a shared parking lot, a maximum of 60 percent of the required parking spaces assigned to one use may be counted or assigned to another use(s) if the uses operate at different peak times.

3. General Provisions. An applicant proposing the use of shared and/or joint use parking shall:

- a. Demonstrate that the express legal right to use the parking spaces in question has been obtained,

- b. Provide a shared and/or joint use parking agreement detailing how the parking spaces will be allocated to or jointly used by the uses. The agreement shall be recorded with the Waukesha County Register of Deeds upon approval,
- c. Sign an acknowledgement that the continuing validity of the applicant’s development permit depends upon the continuing ability to provide the requisite number of parking spaces. If the shared and/or joint use parking is no longer available or the property owner fails to comply with the requirements of this Subsection, the parking requirement reverts to those requirements found in Table 22.06(4)(B)(3) and the property owner or applicant must demonstrate compliance with said requirements, or the development permit shall be revoked, and
- d. Submit an updated shared and/or joint use parking agreement should there be a change in owner, change in use, or expansion or reduction in building or parking lot area.

E. Parking Study. A parking study shall include estimates of parking demand based on recommendations of the Institute of Traffic Engineers (ITE), or other acceptable estimates as approved by the Community Development Director or their designee and should include other reliable data collected from uses or combinations of uses that are the same as, or comparable with, the proposed use. Comparability will be determined by density, scale, bulk, area, type of activity, and location. The study shall document the source of data used to develop the recommendations.

F. Parking Space and Drive Aisle Dimensional Requirements. All parking spaces and drive aisles shall meet the minimum dimensional requirements detailed in Table 22.06(4)(F).

Table 22.06(4)(F) Minimum Parking Space and Drive Aisle Dimensional Standards [1]				
Parking Space Layout	Parking Space Width (A)	Parking Space Length (B)	One-Way Drive Aisle Width (C)	Two-Way Drive Aisle Width (D)
90 Degree	9 ft	18 ft	12 ft	24 ft
60 Degree	9 ft	20 ft	14 ft	n/a
45 Degree	9 ft	19 ft	12 ft	n/a
30 Degree	9 ft	17 ft	12 ft	n/a
Notes				
[1] One parking space within a residential driveway may be counted, provided it measures 8 feet by 18 feet and does not obstruct sidewalks, accessways, or required sight distance.				

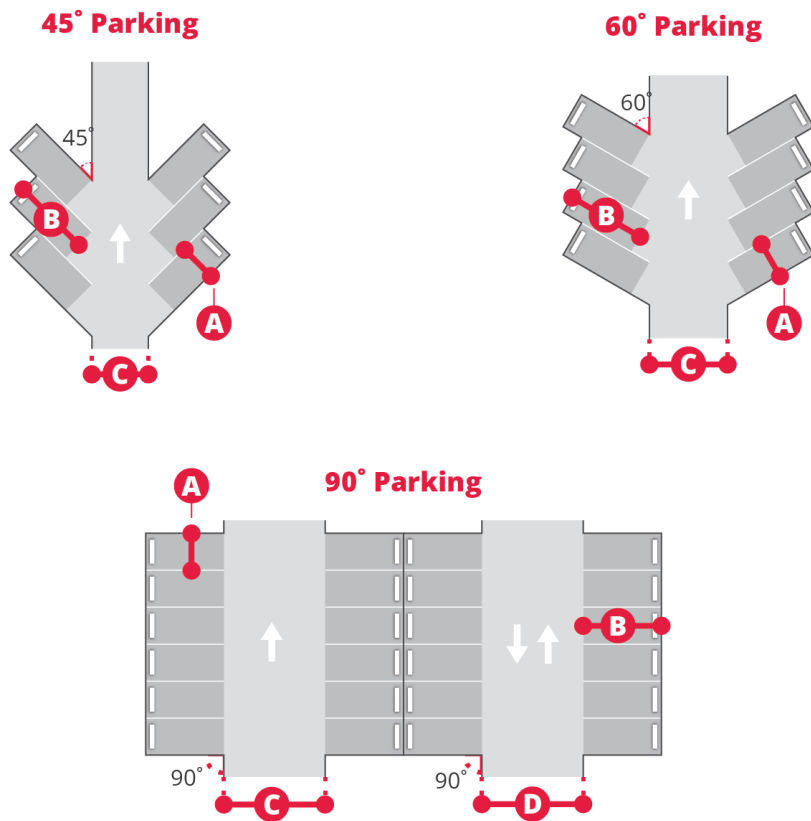


Figure 4 Drive Aisle Dimensions

G. Parking Lot Design Standards.

1. **Surfacing Materials.** All parking lots shall be hard surfaced with concrete or asphalt meeting the requirements of the City's Development Guideline unless otherwise approved by the Director of Public Works or their designee as a permeable surface.
2. **Reserved Parking Spaces.**
 - a. **Accessible Parking.** All development shall provide accessible parking in accordance with the Americans with Disabilities Act, as amended, regardless of whether vehicle parking is required by the City.
 - b. **Short-Term Parking.** Parking spaces reserved for curb-side pickup patrons, rideshare pick-up and drop-off, or other short-term parking needs, shall be located away from main building entrances, near employee exits, or in another location approved by the Community Development Director or their

designee, so as not to inhibit pedestrian travel between the principal building entrance and parking lot.

3. **Vehicular Connectivity.** Adjoining parking lots serving (or potentially serving) any use in a use category other than the neighborhood residential use category or industrial and automotive use category shall be interconnected as follows:
 - a. At least one vehicular connection shall be provided at all lot lines that are coincident for at least 60 feet with another applicable property.
 - b. The vehicular connection shall:
 - i. Be at least 20 feet wide,
 - ii. Align with a vehicular connection that has been previously constructed on an adjacent property or shall be stubbed for future vehicular connection if the adjacent site is undeveloped or does not include a vehicular connection stub,
 - iii. Not be placed where a building on an adjacent property is within 50 feet of the lot line which would hamper traffic movements within the parking lot, and
 - iv. Be placed in an area which will not require the removal of significant natural features such as wetlands or trees with a caliper of 20 inches or more.
 - c. Where a vehicular connection is required, an easement for ingress and egress to adjacent lots shall be recorded by the property owner with the Waukesha County Register of Deeds in the form of an easement plat.

22.06(5) Bicycle Parking.

A. Minimum Bicycle Parking Requirements.

1. **Minimum Short-Term Bicycle Parking Requirements.** All nonresidential development and nonresidential portions of mixed-use developments shall provide short-term bicycle parking in accordance with Table 22.06(5)(A).

Table 22.06(5)(A) Minimum Short-Term Bicycle Parking Requirements	
District	Minimum Required Number of Bicycle Parking Spaces
RN-1, RN-2, RN-3, RN-4	1 per 40 vehicle parking spaces or 6, whichever is greater
MU, DC	1 per 60 vehicle parking spaces or 4, whichever is greater
GC, CC, I	1 per 60 vehicle parking spaces or 4, whichever is greater
LM, GM, MM, OM	1 per 80 vehicle parking spaces or 2, whichever is greater

2. **Minimum Long-Term Bicycle Parking Requirements.** All multi-unit residential uses with five or more units, with the exception of foster family

homes, nursing homes, and community living arrangements, shall provide long-term bicycle parking at a minimum ratio of one bicycle parking space per 20 required vehicle parking spaces or eight bicycle parking spaces, whichever is greater, in all zoning districts.

B. Bicycle Facility Standards.

1. General Bicycle Facility Standards.

- a. Surfacing.** Bicycle parking, except for long-term indoor bicycle parking facilities, shall be provided on a hard-surface, all-weather pavement of asphalt or concrete.
- b. Placement.**
 - i.** Proposed bike parking facilities shall comply with the following spacing standards:
 - 1.** Side-by-Side Rack. Four-foot separation.
 - 2.** End-to-End Rack. Five-foot separation.
 - 3.** Other Rack Type. As approved by the Director of Community Development.
 - ii.** Bicycle parking facilities shall be located three feet away from walls, fences, and the edge of landscaping at the time of maturity, as measured from the edge of the rack closet to the wall, fence, or landscape area.
 - iii.** Bicycle parking facilities adjacent to a pedestrian walkway shall be sited to ensure that a minimum five-foot walkway clearance is maintained at all times.
 - iv.** If provided outdoors, bicycle parking facilities shall be sited within 50 feet of a primary entrance of a building.

- v. If provided indoors, bicycle parking facilities shall be located within an easily accessible common area designated for secure bicycle storage.

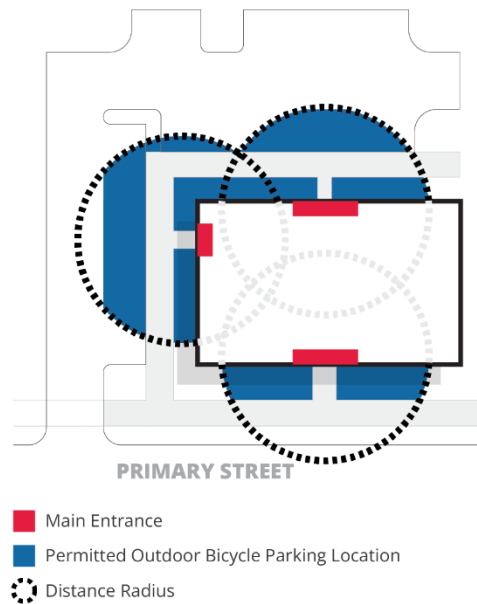


Figure 5 Bicycle Facility Standards

c. Rack Types.

- i. Bicycle racks shall meet all requirements of the Americans with Disabilities Act. Inverted U or Post and Ring bicycle racks are preferred.
- ii. Grid, wave, and spiral bicycle racks are prohibited.
- iii. Bicycle lockers or similar types of facilities are permitted and count towards required bicycle parking standards.

2. Long-Term Bicycle Facility Standards.

- a. Long-term bicycle parking facilities shall be covered and weather resistant.
- b. Storage within a dwelling shall not be considered a long-term bicycle parking space.

3. Parking Structures.

- a. Parking structures shall provide bicycle parking within the structure.

- b. Bicycle parking shall be located on the level closest to the street and/or a pedestrian entrance.

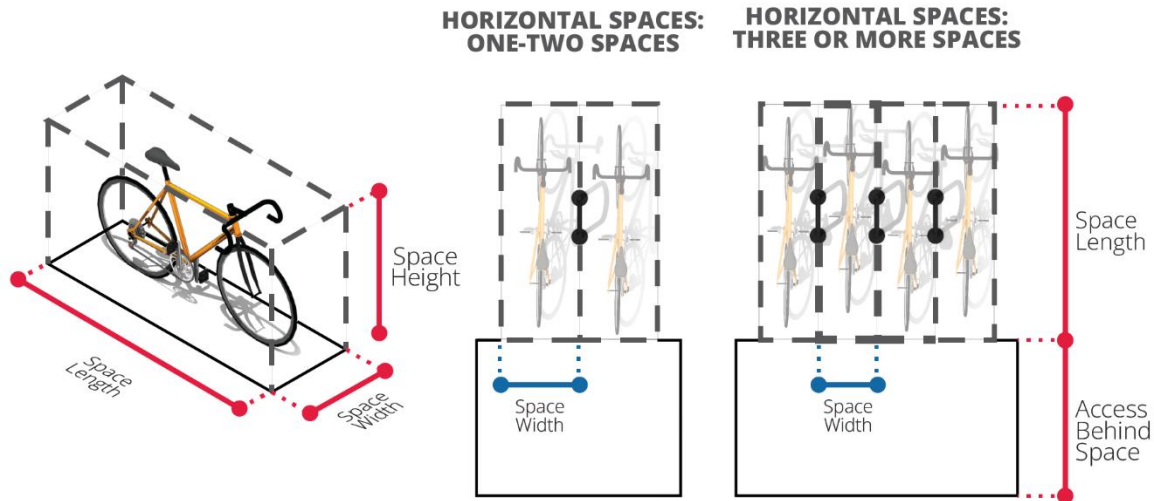


Figure 6 Bicycle Parking Dimensions

22.06(6) Pedestrian Walkways and Trails.

A. On-Site Pedestrian Walkways.

1. **Required Connections.** On-site pedestrian walkways shall connect:
 - a. All buildings on a development site to one another,
 - b. Parking lots to building entrances and internal access drives,
 - c. Each building on a development site to social gathering space located within the development site,
 - d. Building entrances to transit stops on or within the right-of-way fronting a development site, and
 - e. Building entrances to a public sidewalk. In cases where a building is not adjacent to a public sidewalk it must connect to an internal sidewalk, which connects to a public sidewalk.
2. **Pedestrian Walkway Design.**
 - a. On-site pedestrian walkways shall comply with the Americans with Disabilities Act and be clearly marked and delineated.

- b. Where driveways, parking, and loading entrance and exits cross pedestrian walkways, the pedestrian walkway shall be designed with minimal disruptions to safe, continuous pedestrian connectivity.

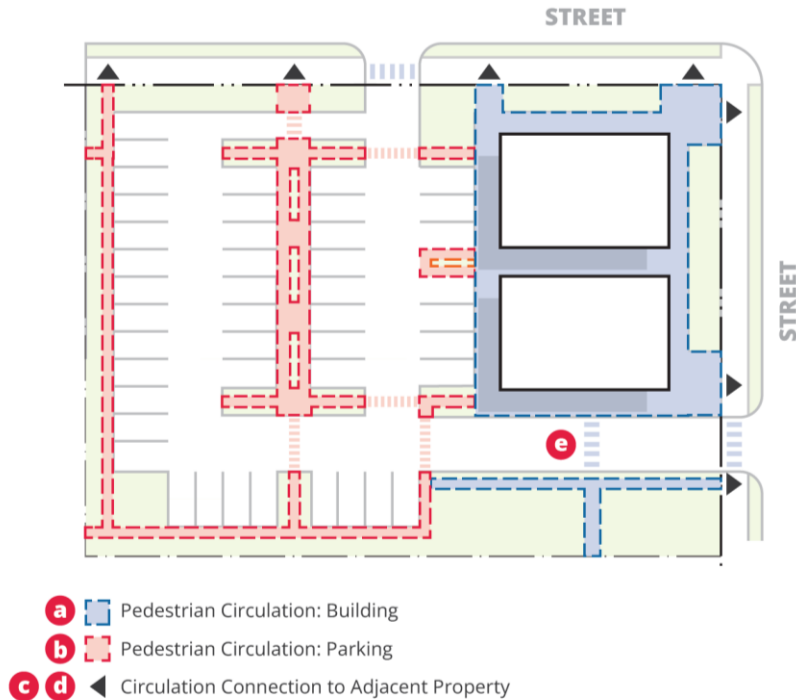


Figure 7 On-Site Pedestrian Walkways

- B. Trails.** Where existing or planned trails, as identified in the Comprehensive Plan or other City- approved plans are adjacent to or traverse a development site, the developer shall:
1. Provide at least a 20-foot public trail easement or outlot and construct the trail in accordance with the standards established in the City’s Development Guideline found in the Handbook. The Director of Public Works or their designee may approve alternative widths when factors such as topography, environmental, safety or available space warrants adjustment.
 2. If the trail is not constructed, the developer shall provide a payment in lieu of construction to the City as part of the Development Agreement.